



TECHNICAL MEMORANDUM #3

Date: May 9, 2023 Project #: 23021.050
To: Project Management Team
From: Kittelson & Associates, Inc.
Project: Curry County Transportation System Plan Update
Subject: Final Tech Memo #3: Update System Inventory (Task 4.1)

INTRODUCTION

This memorandum provides an inventory of Curry County's land uses and transportation facilities and services as they exist today. This information provides a baseline understanding of current gaps and deficiencies in the transportation system and a foundation for evaluating existing and forecast traffic conditions. The findings about the transportation system that are presented in this memorandum will be used to help inform policies, projects, programs, and studies recommended in the Curry County Transportation System Plan (TSP) update.

The TSP addresses transportation needs for people walking, biking, taking transit, and driving within the study area – primarily the regions of Curry County located outside the Brookings, Gold Beach, and Port Orford Urban Growth Boundaries (UGBs), as shown in Figure 1. However, transportation needs along County and State high-order roads within these UGBs, as well as the unincorporated areas of Langlois, Carpenterville, Agness, Wedderburn, and Harbor, are considered. The inventory incorporates the street network and its jurisdictions, functional classifications, and general street characteristics. It also includes information on:

- Bridges and culverts;
- The bicycle and pedestrian network;
- Public transit;
- Rail, water, and pipeline transportation; and,
- Title VI, environmental justice, and environmental considerations.

Information summarized in this memorandum was obtained and assembled using available Geographic Information System (GIS) data, aerial imagery, and field review. Much of the inventory is presented in figures and tables and is supplemented with text.

The Oregon Transportation Planning Rule (TPR – Reference 1) indicates that the study of roadways and intersections is generally limited to those with the highest classification, arterials and collectors. However, local street needs, such as street connectivity and safety are also discussed where appropriate. The executive summary following Figure 1 summarizes the key findings identified through the existing transportation inventory.

The information provided in this memorandum addresses the requirements identified in Oregon Administrative Rule 660-012-020 (Elements of a Transportation System Plan) for providing a general assessment of existing transportation facilities and services.




-  City Boundaries
-  Parks
-  State Parks
-  UGBs
-  Bureau of Land Management
-  US Forest Service



Figure 1

**Study Area
Curry County, Oregon**

EXECUTIVE SUMMARY

Key findings from the existing transportation inventory within Curry County that are presented within this memorandum are summarized below.

Land Use Findings

- A vast majority of land is designated for Timber and some is dedicated to Forest Grazing. Some land outside of UGBs is dedicated to residential development.
- A majority of vacant and re-developable land is zoned as forest or agriculture, which cannot be developed or redeveloped.

Roadway Network Findings

- Roadways are owned by the County, Oregon Department of Transportation (ODOT), Oregon Department of Forestry (ODF), US Forest Service (USFS), Bureau of Land Management (BLM), and incorporated cities; 37% of roads are owned by USFS (most within the Rogue River-Siskiyou National Forest).
- The county lacks major east-west roadway connections. USFS roads that might be considered for improving this connectivity include Hunter Creek Road, Agness Road, Galice Creek Road, Sawtooth Forest Road, and Elk River Road.
- There are discrepancies in the Federal and County functional classifications of Sixes River Road, Grassy Knob Road, China Mountain Road, Old Coast Road, Grizzly Mountain Road, Elk River Road, Vista Drive, Cemetery Loop Road, Lobster Creek Road, Road 3680, Agness Road, Rainbow Rock Road, Parkview Drive, Old County Road, W Hoffeldt Lane, E Benham Lane, Pedrioli Drive, and N Bank Chetco River Road.
- US 101, OR 250, and OR 255 have freight length and width restrictions and are not authorized to move triples combinations. US 101 includes 2 weight restricted bridges. OR 250 and OR 255 are not authorized to continuously move 14 feet wide mobile homes / modular building units.
- None of the State highways are designated by the Oregon Highway Plan (OHP) as freight a route, but US 101 is designated as a Reduction Review Route. US 101 is also a National Highway System (NHS) route but is not designated as a National Highway Freight Route or State freight route on the National Network, except for the portion north of Port Orford.
- The County's rural roadway improvement standards do not specify a minimum width for paved shoulders; in turn, much of the County's rural roadway network lacks paved shoulders and many roadways are substandard (96% of County roadways have paved shoulder widths of 2 feet or narrower).
- Most of the County's arterial and collector network complies with its required access spacing, except in some instances along Cedar Valley Drive, N Bank Rogue River Road, Hunter Creek Road, Pistol River Loop Road, Cape Ferrelo Road, N Bank Chetco River Road, Ocean View Drive, Winchuck River Road.

- Most County and State highways have fair pavement condition or better. Roadways with sections having poor or worse pavement conditions include Hensley Hill Road, OR 251, US 101 near Gold Beach and Brookings, other roadways in Brookings, Gold Beach, and Port Orford, and OR 255.
- Most State and County facilities are two-lane roadways, including US 101, except in and around Brookings and Gold Beach. US 101 includes several stretches of passing lanes and pull-outs.
- US 101 is generally 55 MPH, except through major urban areas. Most major County facilities are 45 to 55 MPH, but sections of N Bank Rogue River Road, N Bank Chetco River Road, S Bank Chetco River Road, and Oceanview Drive are 40 MPH or slower.
- Most public street intersections are stop sign controlled or uncontrolled. The only known signalized intersections are along US 101 in Brookings and north of Gold Beach.

Bridges and Culverts Findings

- The County maintains 36 of the 75 bridges in the county – of which, 32 are on the NBIS – and 3,573 of the 3,600 or more culverts.
- No bridges are functionally obsolete or need review for EV loads, but 6 bridges are structurally deficient, 7 bridges are weight restricted / load posted, 25 bridges have sufficiency ratings below 50, 25 bridges need review for SHV loads, and 47 bridges and 23 culverts are scour critical.

Bicycle and Pedestrian Network Findings

- County collectors and arterials lack walking and biking facilities in both the rural areas and within the UGBs, except for some sidewalks and bike lanes on select streets in Brookings and Gold Beach.
- The only walking and biking facilities available on the State highway system are four foot (or wider) paved shoulders along US 101 (this highway is a coastal bike route). Sidewalks and bike lanes are generally provided on US 101 within the UGBs.
- The Oregon Coast Bike Route (OCBR) is a popular bike route running the length of the Oregon coast for approximately 370 miles. The Wild Rivers Coast Scenic Bikeway is an Oregon Scenic Bikeway in the Port Orford area.
- Many of Curry County's primary activity centers that could generate biking and walking trips are located within the urban and unincorporated areas and appear to be accessible by bicycle or walking.

Public Transit Findings

- Public transit is operated by Curry Public Transit (CPT) and the SouthWest POINT. CPT runs fixed-route service between Coos Bay/North Bend and Smith River and dial-a-ride service in Brookings and Gold Beach. SouthWest POINT operates intercity bus service between Klamath Falls and Brookings.
- CPT has 5 official bus stops in Port Orford, Gold Beach, Brookings, and Harbor and 2 flag stops in Langlois. Bus stop amenities range from no amenities to covered shelters, etc. CPT does not have park and ride facilities or transit centers in its service area.
- 24% of Curry County's overall population lives with a disability and many are concentrated around the incorporated cities and unincorporated rural communities.
- The recently adopted CPT Transit Development Plan (TDP) has identified several transit needs that will be considered throughout the TSP Update.

Rail Findings

- No rail transportation is present within Curry County. The closest railroads are Central Oregon & Pacific Railroad (CORP) to the east and Coos Bay Rail Line (CBRL) to the north.

Air, Water, and Pipeline Findings

- Three general aviation public airports serve Curry County: the Brookings Airport, the Gold Beach Municipal Airport, and the Cape Blanco State Airport. Del Norte Airport in Crescent City, California also supports populations in the county.
- The Brookings Airport and the Cape Blanco State Airport are classified as Tier 3 and Tier 1 airports, respectively, in the Oregon Aviation Plan (OAP). These tiers reflect their capabilities and for making future investments, based on the forecast needs of their surrounding population and their provision of economic and commercial restoration to the entire region after a disaster event. These airports are also classified as Tier 2 airports in the Oregon Resilience Plan (ORP), which is a tier that represents a larger network of airports (compared to a more limited number of Tier 1 airports) that provide access to most rural areas and will be needed to restore major commercial operations following a natural disaster.
- The Brookings Airport and Cape Blanco State Airport have "severe" and "violent" Cascadia Event Hazard rankings, respectively, but both are located outside of known tsunami hazard areas. The Gold Beach Airport has a "violent" Cascadia Event Hazard ranking and is located inside known tsunami hazard areas.
- Three ports are in Port Orford, Gold Beach, and Brookings that support recreational and commercial fishing and provide a variety of other services. They are not defined as part of the statewide strategic freight network but should be considered for their role in the transportation network.
- There are no known pipelines.

Title VI and Environmental Justice Findings

- 17% of the population is of a minority race or ethnicity and the highest concentration resides within the Brookings UGB.
- 34% of the population are those aged 65 or older and the highest concentration is south of Harbor (75% of that population).
- 13% of the population is in poverty. The Census Block Groups in northeast county and southeast of Brookings have the highest percentage of households with a poverty ratio below 2.

Environmental Constraint Findings

- Curry County is rich in Goal 5 resources, being home to 152 National Register and 150 State Historic Preservation Office (SHPO) historic resources (105 are considered eligible / significant) and having large areas of undeveloped / public lands that support wildlife.
- There are several Federal Emergency Management Agency (FEMA) mapped floodways and floodplains.
- There are numerous federally listed endangered plant and animal species as well as critical habitat and state listed species, species of concern, and sensitive species.
- There are no known superfund sites but there are hundreds of known contaminated sites and spills that have occurred.
- Curry County is located on the ancestral lands of the Cayuse, Umatilla, and Walla Walla tribes; Tolowa Dee-ni'; Chit-dee-ni (Chetco); Coquille Indian Tribe; and the Confederated Tribes of the Siletz Indians.

LAND USES

This section identifies the Comprehensive Plan and zoning map designations that help define land use within Curry County. This section also provides information on existing land uses and vacant and developable lands within the county. Land use is a key factor in developing a functional transportation system; the amount of land planned for development, the types of land uses, and how they relate to each other have a direct relationship to the anticipated demands for the transportation system.

Comprehensive Plan Designations

The Curry County Comprehensive Plan, last updated in 2009, provides a long-term guide for where and how future development will occur. The Comprehensive Plan designations inform which zoning districts can be applied to the land. Table 1 describes the Comprehensive Plan designations within Curry County.

Table 1. Comprehensive Plan Designations (2009)

Plan Designation	Description
Timber	Commercial forest lands have been placed in a Timber zone that allows commercial forestry uses and other compatible uses exclusively.
Forest Grazing	The Forestry Grazing designation is applied to lands which either have a combination of agricultural and forest uses or have capability for either use. Therefore, each parcel must be considered in terms of whether the land is predominately suitable for agricultural or forestry use.
Agriculture	The Comprehensive Plan for Curry County recognizes the following aspects of agriculture in the county: 1) those lands which have agricultural capabilities; 2) the nature of agricultural use applied to those lands; and 3) the existing parcelization of those lands. The Comprehensive Plan also recognizes two areas in the county with unique agricultural characteristics as farm districts. The remaining agricultural land in the county is grazing and pastureland which is treated as a separate agricultural unit.
Residential	The County inventoried and identified buildable land suitable for residential development in order to encourage the development of a variety of housing types to meet the needs of Curry County residents. Existing residential lands are mainly found along US 101 along the coast.
Commercial	Commercial lands fuel the economy of Curry County. These lands are designated for rural, light, and heavy commercial uses. Rural commercial uses are uses that can be developed on individual wells and septic systems. Light commercial uses serve the needs of nearby residences, while heavy commercial facilities are more like storage facilities or light manufacturing activities.
Industrial	The industrial zone provides for those uses which are intensive manufacturing and processing uses which could be in conflict with residential or commercial uses.
Special Use Lands	Special Use Lands include coastal conservation areas, estuarine resources, and public facilities.
Open Space	Open spaces are areas that are to remain structurally undeveloped other than for support facilities. These may include both designated open space areas such as parks, playgrounds, golf courses, etc., or public and private lands in use for agriculture, forestry, open area recreation and so on.

Table 2 shows the acreage of land within Curry County by Comprehensive Plan Designation. As shown, a vast majority of land is designated for Timber. A smaller portion of land is dedicated to Forest Grazing (eight and a half percent), and approximately one and a half percent of land outside of UGBs is dedicated to residential development. Timber and grazing uses are limited in the amount of development or impact on the TSP. Most of the residential, commercial, and industrial land within Curry County is concentrated around existing communities along US 101.

Table 2. Summary of Plan Designations¹

Plan Designation	Acreage	Percentage
Timber	929,500	88.0%
Forest Grazing	93,400	8.8%
Agriculture	6,000	0.6%
Residential	15,800	1.5%
Commercial	600	0.1%
Industrial	300	0.0%
Special Use	11,100	1.0%
Total	1,056,700	100%

Existing Land Uses

An inventory of existing land uses was produced using Curry County tax assessor property classification data for tax lots within the county. Each tax lot is classified in accordance with Oregon Revised Statute (ORS) 308.215. With the exception of specially assessed properties, the classification is based upon the highest and best use of the property. Tax assessor information for tax lots within the county provides a basic inventory of current land uses. Table 3 summarizes this information based on number of parcels and acres of land designated for a certain use.

Table 3. Parcel Use Classification

Use Classification	Parcel Count	Total Acreage
Miscellaneous	23	0.85
Residential	306	213.76
Commercial	35	8,437.85
Industrial	0	0.00
Tract	134	1,514.24
Farm/Ranch	81	13,787.45
Forest	252	44,232.31
Multifamily	1	0.69
Recreational	1	0.75
Exempt	122	74,372.85
Unclassified	46	0

Source: Curry County Tax Assessor Data

Zoning Designations

Land use regulations are implemented through the Curry County Zoning Ordinance (amended September 2022). The following is a summary of the existing zoning in the county and the associated requirements that govern development. This overview is intended to provide an indication of the type and intensity of land uses that can be expected within the planning horizon to determine future traffic demand. The County's zoning requirements establish allowed uses and associated development regulations, summarized in Table 4.

¹ Excluding Incorporated Cities and their UGA (about 8,300 acres). Source: 2009 Comprehensive Plan

Table 4. Land Use Summary (2022)

Zone	Zone Description	Parcel Count
AFD	Agriculture	13
C-1	Light Commercial	1
C-2	Heavy Commercial	1
CON	Beach & Dune Conservation Area	20
EFU	Exclusive Farm Use	18
FG	Forestry Grazing	356
I	Industrial	1
PF	Public Facility	12
R-1	Residential One	1
R-3	Residential Three	1
RC	Rural Commercial	67
RCR-1	Rural Community Residential – 1 Acre	53
RCR-10	Rural Community Residential – 10 Acre	7
RCR-2.5	Rural Community Residential – 2.5 Acre	8
RCR-5	Rural Community Residential – 5 Acre	23
RI	Rural Industrial	9
RR-10	Rural Residential – 10 Acre	65
RR-5	Rural Residential – 5 Acre	179
T	Timber	156
NOT ZONED	Not Zoned	9

Source: Curry County GIS Data

Figure 2 identifies the locations of zoning designations throughout the County.² The locations of these zoning designations also implement the County's Comprehensive Plan designations described previously. As such, the patterns of zoning designations and Comprehensive Plan designations within Curry County are similar.

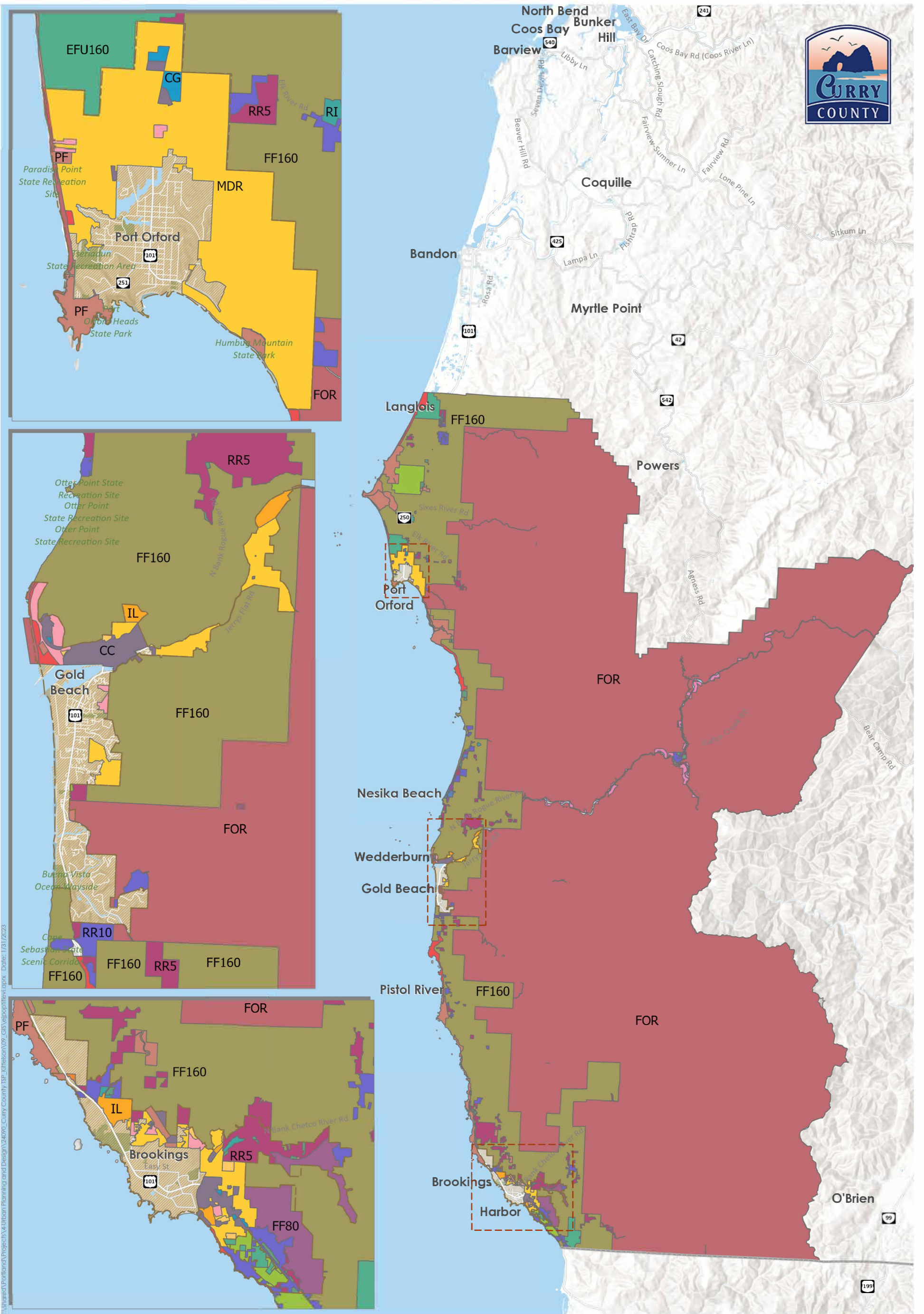
As show in Figure 2, the land around the county's urban areas – i.e., outside Gold Beach, Brookings, and Port Orford – is zoned medium density residential (R-2). Much of the land along US 101 is zoned rural residential (RR) or forestry grazing (FG). The majority of the land within Curry County is related to agriculture and forestry. All of these are limited in the amount of development or impact on the transportation demand. Residential zones comprise the largest amount of land that generates transportation demand in unincorporated areas of the county. Although there is some land zoned for various commercial and industrial uses, there are generally not transportation-intensive uses in unincorporated areas.

Vacant and Developable Land

Tax assessor information can be used to provide a basic inventory of vacant and re-developable land. According to County assessor data, there are currently 128 publicly owned and 522 vacant parcels in Curry County³. For vacant land, the County's property classification data indicates whether there are no improvements on the land for each tax lot.

² The zone designations listed in Table 4 are based on the 2022 redesignation of County land, while the zoning map in Figure 2 is representative of 2014 zoning data. Discrepancies between the map and table are due to lack of updated zoning map data.

³ Data for this assessment was based on the most recent County tax assessor data. Table 5 only shows re-developable parcels, whereas Table 3 shows all parcels.



- | | | | |
|--------|------|------|--------------------|
| BD | FOR | PF | RR5 |
| CC | FUD | PF80 | SF80 |
| CG | IL | RC | UCRC |
| EFU160 | MDR | RI | <all other values> |
| EFU40 | MHDR | RR1 | UGBs |
| FF160 | MLDR | RR10 | County Boundary |
| FF80 | OSC | RR2 | |



Figure 2
Zoning (2014)
 Curry County, Oregon

Land is assumed to be re-developable if the land value is twice the value of assessed improvements on the tax lot. According to this methodology, there are 578 re-developable parcels in Curry County, including vacant parcels, as shown in Table 5. The amount of vacant and re-developable land is likely overestimated as this includes forestry and agriculture-related land that cannot be developed or redeveloped; and does not include deductions for public rights-of-way, private streets, or easements.

Table 5. County Land Use Classification of Re-Developable Parcels

Classification (First Digit)	Parcel Count	Total Acreage
0 – Miscellaneous	6	0.85
1 – Residential	124	140.35
2 – Commercial	11	8,386.99
3 – Industrial	0	0.00
4 – Tract	78	1,336.9
5 – Farm/Ranch	76	13,646.2
6 – Forest	240	44,124.48
7 – Multifamily	0	0.00
8 – Recreational	1	0.75
9 – Exempt	42	13,094.25
Total	578	80,730.77

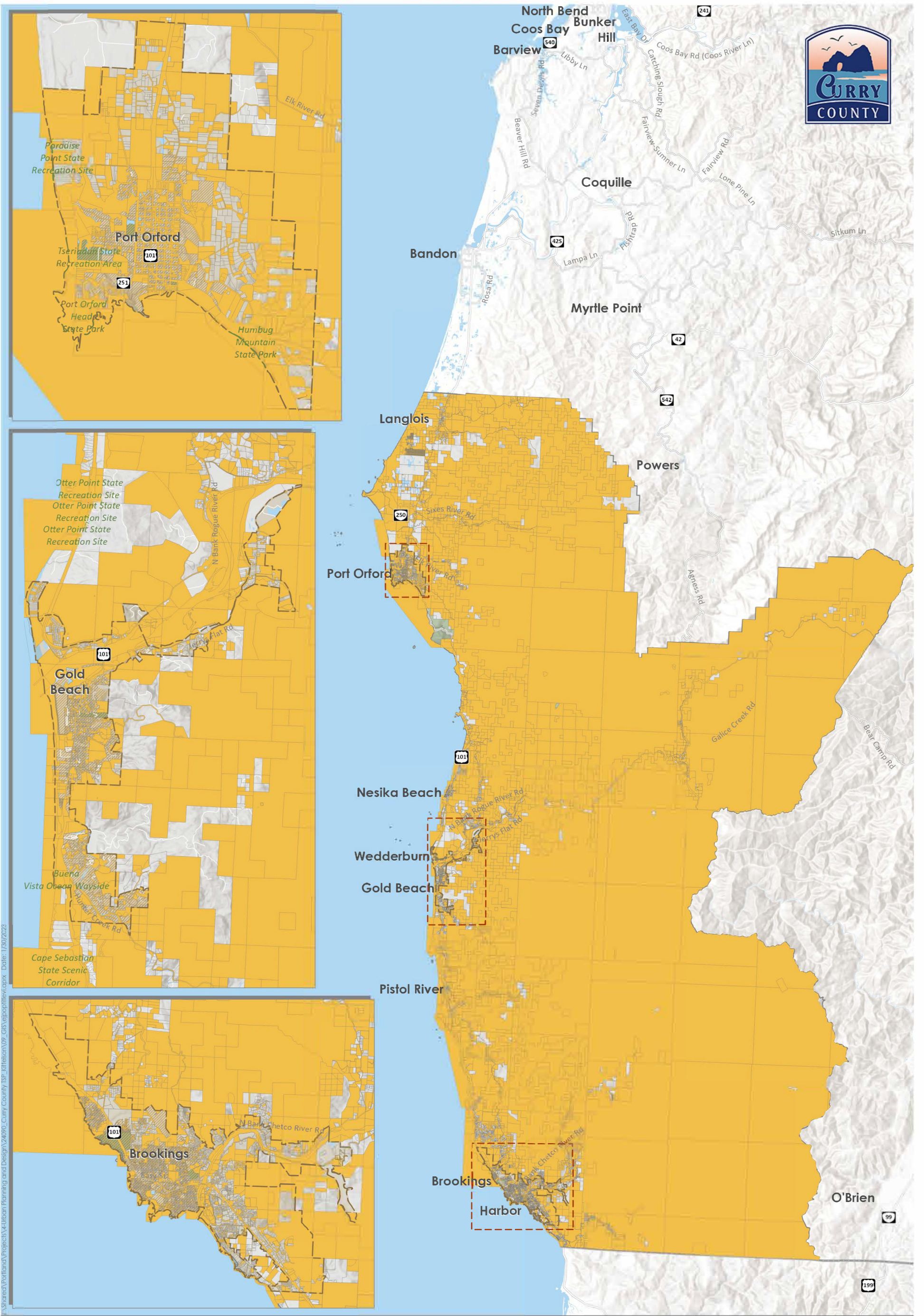
Source: Curry County Tax Assessor Data

County parcel data was further categorized by zip code to identify the geographic location of unimproved or re-developable parcels in the county. According to this analysis, there are 75 re-developable parcels in the Port Orford area (97465 zip code) and 35 re-developable parcels in the Gold Beach area (97444 zip code). This data lacks the detail needed to determine if these parcels are inside or outside of UGBs and, thus, in County jurisdiction. The remainder of the potentially re-developable parcels in the data do not have record of a zip code.

Figure 3 is a visualization of vacant and re-developable land recorded as GIS data, maintained by the Lane Council of Governments (LCOG). As indicated by the gold-colored parcels in Figure 3, there are many unimproved parcels in Curry County. Most of this land is forestry or agriculture-related land and would not be developable or re-developable. Further analysis of parcel classification data overlaid with zoning data shows that while the majority of the unimproved parcels are not re-developable because they are agriculture-related or forestry land, there is a concentration of re-developable parcels zoned for rural residential development outside each of the UGBs in the county.

Land Use Findings

- The zoning designations and Comprehensive Plan designations within Curry County follow similar patterns.
- A vast majority of land is designated for Timber and a smaller portion is dedicated to Forest Grazing. A small percentage of land outside of UGBs is dedicated to residential development.
- A majority of vacant and re-developable land in the county is zoned as forest or agriculture, which cannot be developed or redeveloped (some re-developable parcels are in the Port Orford and Gold Beach areas).






-  UGBs
-  County Boundary
-  Unimproved Parcels



Figure 3
**Unimproved Parcels
 Curry County, Oregon**

Shared\Public\Projects\Urban Planning and Design\24090_Curry County\19_10\101Urban109_CasVejo\ppl\view\cas Date: 1/20/2022

ROADWAY NETWORK

The roadway network in Curry County is vital to the transportation system. Motor vehicle, bicycle, pedestrian, transit, and freight transportation all rely on the roadway network to some extent. Beyond these modes, the roadway network also provides access to air, water, and pipeline facilities within the county. The following sections describe the various characteristics of the roadway network including jurisdictions, functional classifications, freight route designations, roadway improvement standards, access spacing standards, and other general traits.

Jurisdiction

Roads within Curry County are owned and maintained by multiple jurisdictions, as illustrated in Figure 4, including:

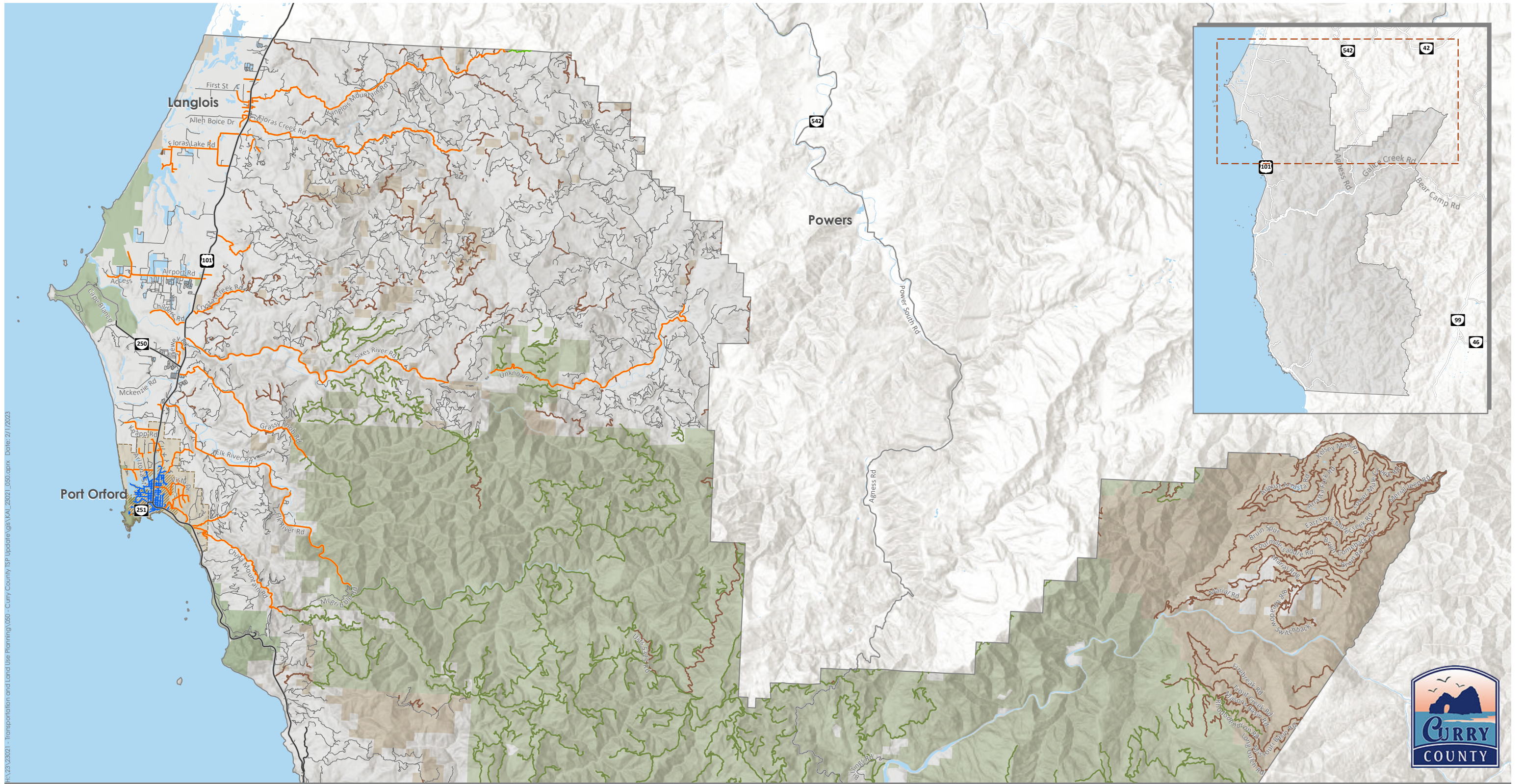
- Curry County
- Oregon Department of Transportation (ODOT)
- Oregon Department of Forestry (ODF)
- US Forest Service (USFS)
- Bureau of Land Management (BLM)
- Cities of Brookings, Gold Beach, and Port Orford

Each jurisdiction is responsible for determining the roadway's functional classification, defining its major design and multimodal features, and approving construction and access permits. Coordination is required among the jurisdictions to ensure that roads are planned, operated, maintained, and improved to safely meet public needs.

County Roads

Curry County owns and maintains approximately 230 miles of road system that was mostly acquired by the year 1970. Eighty-five percent of the road system is paved, and the remainder is Oil Matt (four percent) and gravel (11 percent). Of the 230-mile roadway system, 130 miles are primarily used to access forest land. The major County roads in the study area include:

- | | | |
|--------------------------------|--------------------------------|--------------------------|
| • Winchuck River Road | • North Bank Pistol River Road | • Cedar Valley Road |
| • Oceanview Drive | • Pistol River Loop Road | • Ophir Road |
| • W Benham Lane | • Oak Flat Road | • Nesika Beach Road |
| • Lower Harbor Road | • Paradise Point Road | • Euchre Creek Road |
| • Shopping Center Avenue | • Hunter Creek Loop Road | • Elk River Road |
| • South Bank Chetco River Road | • Hunter Creek Road | • Sixes River Road |
| • North Bank Chetco River Road | • Jerry's Flat Road | • Airport Road |
| • Gardner Ridge Road | • North Bank Rogue River Road | • Floras Lake Road |
| • Cape Ferrelo Road | • Edson Creek Road | • Floras Lake Loop Road |
| | | • Langlois Mountain Road |



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Roadway Ownership

- ODOT
- County
- City
- Oregon Department of Forestry
- US Forest Service
- BLM



Figure 4

**Roadway Jurisdictions
Curry County, Oregon**



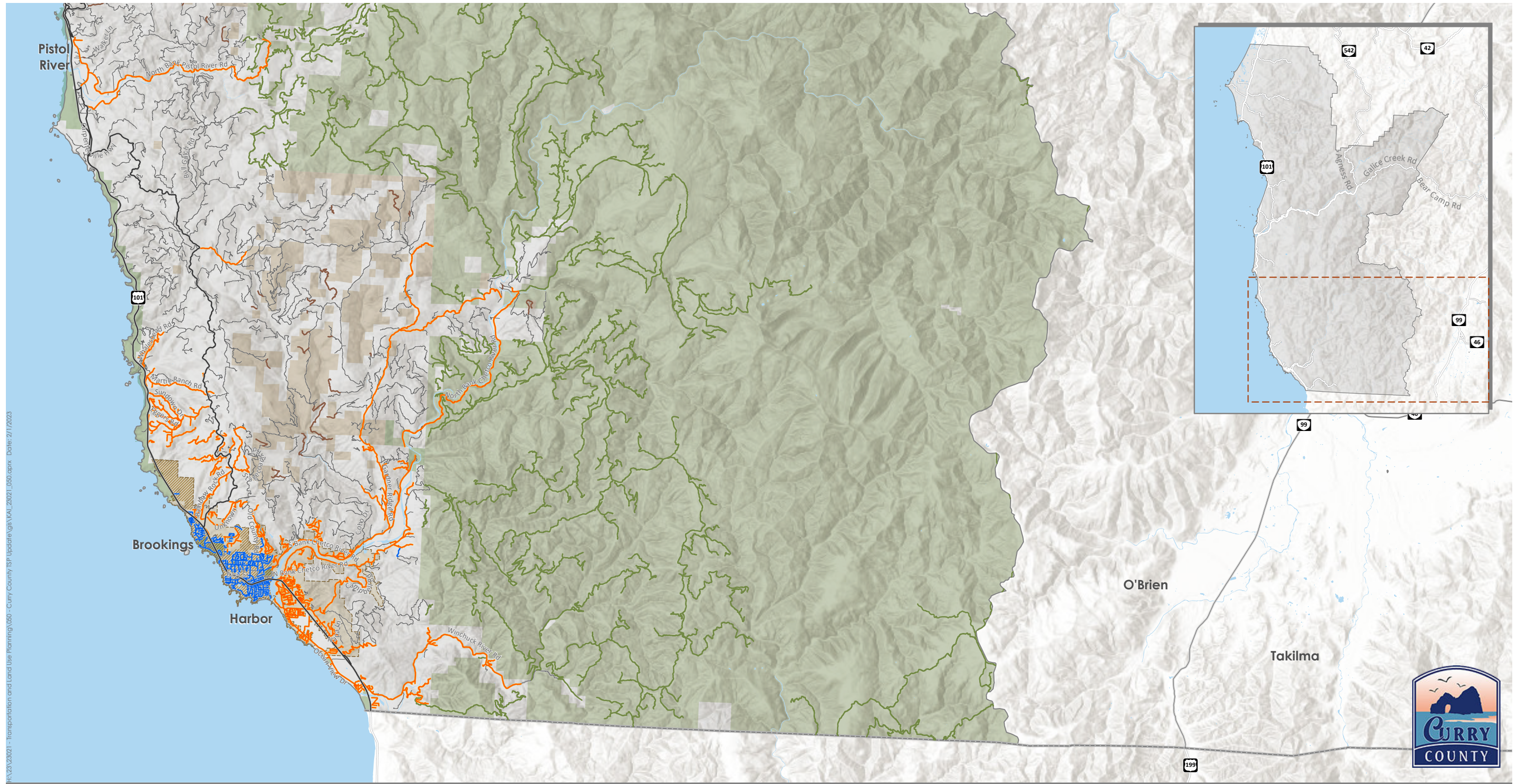
Roadway Ownership

- ODOT
- County
- City
- Oregon Department of Forestry
- US Forest Service
- BLM



Figure 4

**Roadway Jurisdictions
Curry County, Oregon**



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Roadway Ownership

- ODOT
- County
- City
- Oregon Department of Forestry
- US Forest Service
- BLM



Figure 4

**Roadway Jurisdictions
Curry County, Oregon**

Some of the major County roads referenced above cross into the UGBs of the following cities:

- **Port Orford** – Paradise Point Road
- **Gold Beach** – Hunter Creek Road; Hunter Creek Loop Road; Jerry's Flat Road; North Bank Rogue River Road
- **Brookings** – North Bank Chetco River Road; South Bank Chetco River Road; Lower Harbor Road; Shopping Center Avenue; Oceanview Drive; W Benham Lane

US Forest Service Roads

The USFS has jurisdiction over 1,350 miles of the roads in Curry County, which accounts for 37 percent of all known roads in the region. Most of them are located exclusively within the boundaries of the Rogue River-Siskiyou National Forest and their primary function is to provide forest access for logging and recreational activities. The USFS is not a public road agency; therefore, its responsibilities and liabilities for road maintenance are not the same as the State and County. Due to reductions in the amount of logging activity in the Siskiyou National Forest and in the federal budget, numerous roads within the USFS road system have been closed. The priority roads that remain open are those that still have considerable recreational and commercial use.

The USFS classifies its roads using a system of five maintenance levels, which increase in importance with increasing level:

- **Maintenance Level 1** – Assigned to intermittent service roads during the time periods that they are closed to vehicular traffic. Basic custodial maintenance is performed to reduce damage to adjacent resources and to perpetuate the road for future use.
- **Maintenance Level 2** – Assigned to roads open for use by high clearance vehicles. Passenger cars are not considered in the maintenance of these roads. Traffic is normally minor and consists of administrative, permitted, dispersed recreation or other specified uses.
- **Maintenance Level 3** – Assigned to roads open and maintained for travel by prudent drivers in a standard passenger car. User comfort and convenience are not considered priorities in maintenance. Roads in this class are typically low speed, one lane with turnouts and spot surfacing.
- **Maintenance Level 4** - Assigned to roads that provide a moderate degree of user comfort and convenience at moderate travel speeds. Most roads are two lane and aggregate surfaced; however, some roads in this class may be one lane.
- **Maintenance Level 5** - Assigned to roads that provide a high degree of user comfort and convenience. These roads are normally two lane, paved facilities. Some may be aggregate surfaced.

The following system of USFS roads in Curry County provides important connections to adjacent counties through the Rogue River-Siskiyou National Forest that might be used as alternative routes to the State highway system:

- **Hunter Creek (NF 3680) / Quosatana (NF 3313)** – gravel roads suitable for passenger cars that extend north from Hunter Creek Road in Gold Beach to Agness Road (within Curry County).
- **Agness Road / Galice Creek Road / Bear Camp Road** – paved roads suitable for passenger cars that extend east from Jerry’s Flat Road to Bear Camp Road (in Josephine County).
- **Agness Road** – paved / gravel road suitable for passenger cars that extends north from Galice Creek Road (or Bear Camp Road) to Coos County (within Curry County).
- **Sawtooth Foster Road (NF 3340)** – paved / gravel road that extends east from Lobster Creek Road, northeast of Wedderburn, and north to Agness Road (within Curry County).
- **Elk River Road (NF 5325) / Iron Mountain Road (NF 5502) / Barklow Mountain (NF 3353)** – paved / gravel roads suitable for passenger cars that extend east from Elk River Road to Agness Road (within Curry County).

Bureau of Land Management Roads

The BLM owns approximately 300 miles of roadway within Curry County. BLM roads are used primarily for logging, recreational use, and administrative purposes. Some BLM roads connect to County roads and a majority of them are located in the northern part of the county and concentrated in an area east of the Wild Rogue Wilderness in the northeast corner of the county.

State Highways

ODOT owns approximately 124 miles of roadway within Curry County, including some of the most heavily traveled routes, such as US 101, and all of which are paved. ODOT owns and maintains four primary State facilities within the County:

- US 101 (Oregon Coast Highway) is a major two- to five-lane facility that provides regional mobility through a majority of the County’s populated areas. US 101 is the main north-south coastal route through Washington, Oregon, and California, connecting Olympia, WA to Los Angeles, CA. Locally, it is the main route through the county, connecting Brookings, Gold Beach, and Port Orford and unincorporated communities in between.
- OR 255 (Carpenterville Highway) is a two-lane rural highway that is comprised of two sections:
 - A northern section connecting to US 101 near mileposts (MP) 334.87, 337.02, and 337.97, north of Pistol River; and,
 - A southern section connecting to US 101 at MP 339.71 and 341.85, south of Pistol River, to MP 354.83, in Brookings.
- OR 251 (Port Orford Highway) is a two-lane rural highway connecting US 101 in Port Orford to Port Orford Heads State Park.
- OR 250 (Cape Blanco Highway) is a two-lane rural highway connecting US 101 south of Sixes to Cape Blanco.

Intermodal Connections

The 1999 Oregon Highway Plan (OHP – Reference 2) describes intermodal connections as short lengths of roads that connect intermodal facilities to the State highway system. According to Appendix E of the OHP, there are no defined intermodal connections on the National Highway System located in Curry County. As noted in Technical Memorandum #2 (Policy Review), the 2017 Oregon Freight Plan (OFF) defines a statewide strategic freight network. The 2017 Oregon Freight Intermodal Connector System Study, an OFF support document, designates the roads connecting Curry County ports to US 101 as “Tier 3” Intermodal Connectors. These are minor intermodal connectors that serve more of a local or regional need, typically serving fewer than 50 trucks a day in each direction. The OFF does not identify improvements for these connectors.

Functional Classification

A roadway’s functional classification is determined by several factors, including how the facility connects with the rest of the system, the volume of local or through traffic it is expected to carry, and the different types of trips it is expected to support. The functional classification considers adjacent land uses and varying transportation modes that should be accommodated. The public right-of-way for different functional classifications should provide sufficient space for utilities (or through public utility easements) to serve adjacent land uses. Functional classifications help Curry County to reserve future rights-of-way, determine street design, and develop future street improvement projects.

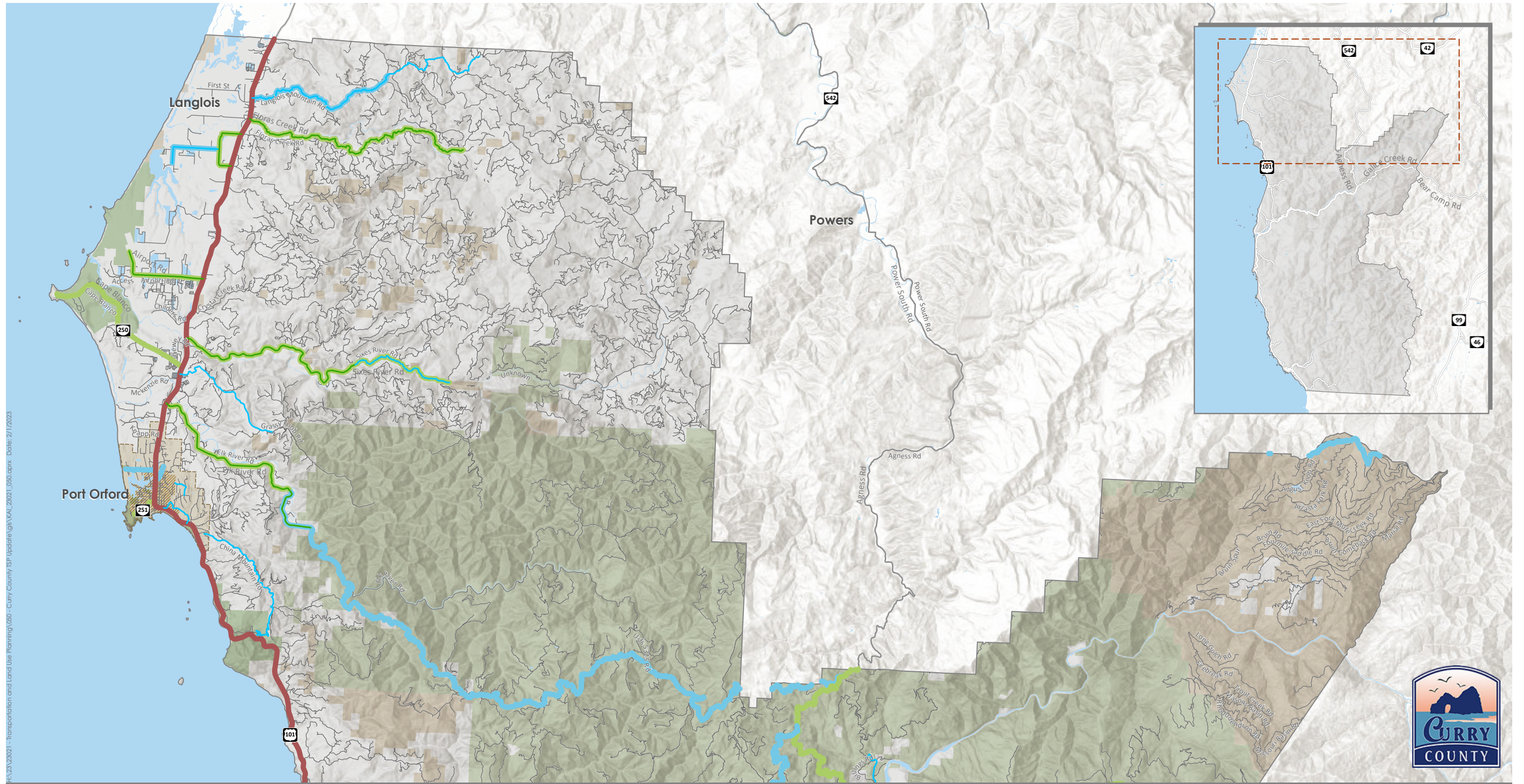
The County, State, and Federal Government define functional classifications for roadways in the county, as described in the following sections. Figure 5 illustrates the County and Federal functional classifications for roadways within Curry County. The State does not define functional classifications for non-State facilities.

County Classifications

According to the Curry County Comprehensive Plan (Reference 3), the County functional classification system divides all County roadways into five broad functional classifications:

- **Principal Arterial** – A roadway with substantial interstate and statewide travel. Principal arterials serve both through traffic and trips of moderate length. Access is partially controlled with infrequent access to abutting properties.
- **Minor Arterial** - A roadway that links cities or land uses that generate large numbers of trips. Travel speeds will be relatively high with minimum interference to through movements.
- **Major Collector** - A roadway providing service to land uses that generate trips such as consolidated schools, shipping points, parks, mining, and agricultural areas. This type of road links minor collectors with streets of higher classification.
- **Minor Collector** - A roadway providing service to small communities. This type of road links locally important land uses that generate trips with rural destinations.
- **Local Road** – A public road that is not a city street, State highway, or federal road. A road connecting the local uses with the collector system. Property access is the main priority; through traffic is not encouraged.

As shown in Figure 4 from the previous section and Figure 5, the county generally lacks major east-west roadway connections between US 101 and the State highway system east of Curry County (e.g., I-5).



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Federal Functional Class

- Other Principal Arterial
- Minor Arterial
- Major Collector
- Minor Collector

County Functional Class

- Rural Minor Arterial
- Rural Major Collector
- Rural Minor Collector

— No Designation



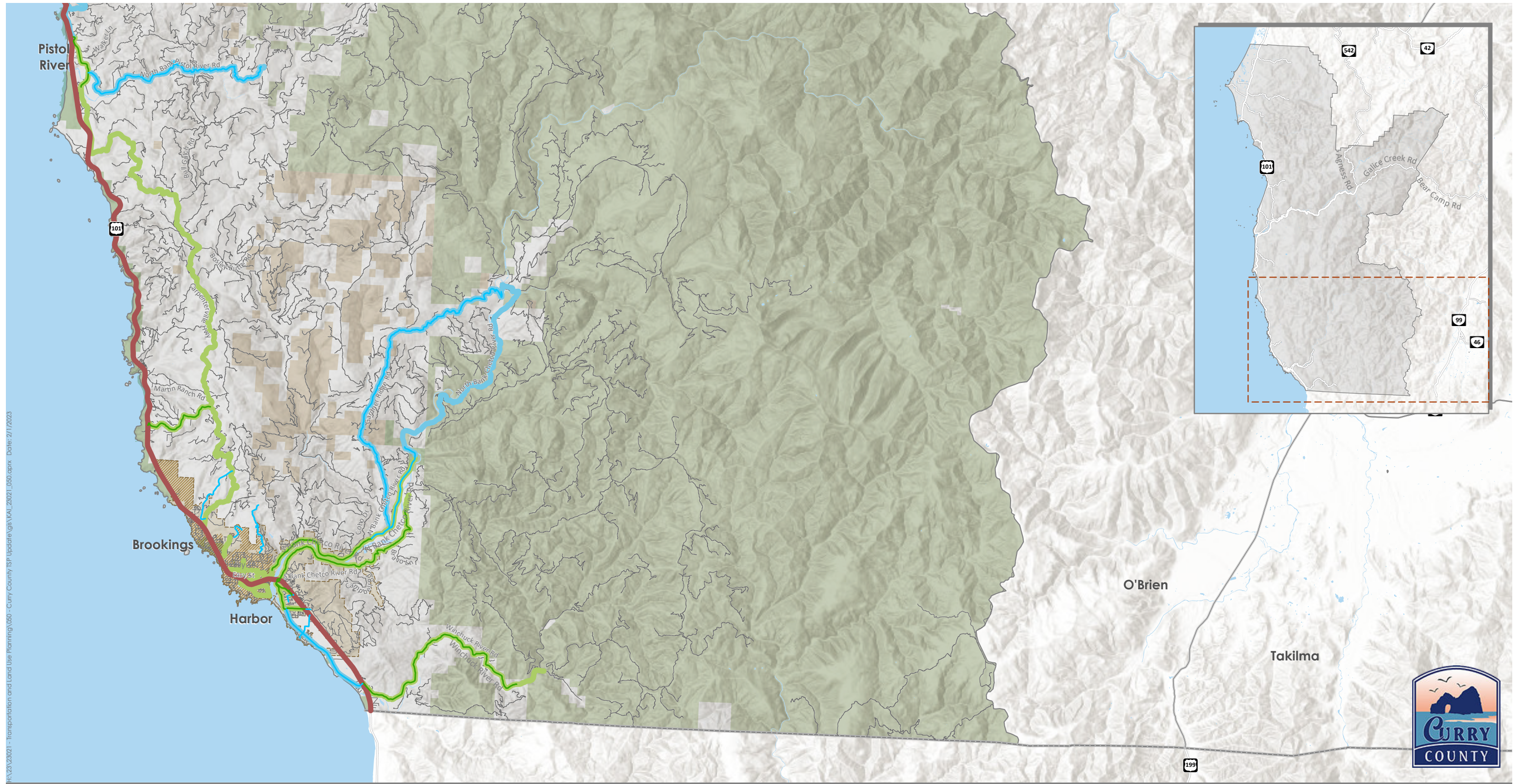
Figure 5

**Functional Classifications
Curry County, Oregon**



Figure 5

Functional Classifications
Curry County, Oregon



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- | | | |
|---------------------------------|--------------------------------|------------------|
| Federal Functional Class | County Functional Class | — No Designation |
| Other Principal Arterial | Rural Minor Arterial | |
| Minor Arterial | Rural Major Collector | |
| Major Collector | Rural Minor Collector | |
| Minor Collector | | |



Figure 5

**Functional Classifications
Curry County, Oregon**

State Classifications

The OHP organizes State highways into five classifications based on function that are used to guide planning, management, and investment decisions regarding State highway facilities. The hierarchy includes Interstate Highways (National Highway System), Statewide Highways (NHS), Regional Highways, District Highways, and Local Interest Roads. Of these five State classifications, these three are relevant to Curry County:

- **Statewide Highways (NHS)** typically provide inter-urban and inter-regional mobility and provide connections to larger urban areas, ports, and major recreation areas that are not directly served by Interstate Highways. A secondary function is to provide connections for intra-urban and intra-regional trips. The management objective is to provide safe and efficient, high-speed, continuous-flow operation. In constrained and urban areas, interruptions to flow should be minimal. Inside Special Transportation Areas (STAs), local access may also be a priority. US 101 is a Statewide Highway.
- **District Highways** are facilities of county-wide significance and function largely as county and city arterials or collectors. They provide connections and links between small urbanized areas, rural centers and urban hubs, and also serve local access and traffic. The management objective is to provide for safe and efficient, moderate to high-speed continuous-flow operation in rural areas reflecting the surrounding environment and moderate to low-speed operation in urban and urbanizing areas for traffic flow and for pedestrian and bicycle movements. Inside STAs, local access is a priority. Inside Urban Business Areas, mobility is balanced with local access. OR 250, OR 251, and OR 255 are all District Highways.
- **Local Interest Roads** function as local streets or arterials and serve little or no purpose for through traffic mobility. Some are frontage roads; some are not eligible for federal funding. Currently, these roads are District Highways or unclassified and will be identified through a process delineated according to OHP Policy 2C (Interjurisdictional Transfers). The management objective is to provide for safe and efficient, low to moderate speed traffic flow and for pedestrian and bicycle movements. Inside STAs, local access is a priority. ODOT will seek opportunities to transfer these roads to local jurisdictions. All other State highways in Curry County not designated as Statewide or District highways are Local Interest Roads.

In addition, the State classifies US 101 as an Oregon Scenic Byway through the county and has designated the sections through downtown Port Orford and Brookings as Special Transportation Areas (STAs), which allow for more traffic congestion.

It is important to note that in the event of a Cascadia Earthquake and Tsunami, the Oregon Resilience Plan (Reference 4) designates US 101 as a Tier 2 highway, which represents a larger highway network across the state that provides access to most urban areas and restores major commercial operations.

Federal Classifications

The Federal Highway Administration (FHWA) provides a rural and urban functional classification system that includes Interstates, Other Freeways and Expressways, Other Principal Arterials, Minor Arterials, Major and Minor Collectors, and Local Roads.

Of the Federal classifications referenced above, the following are relevant to rural Curry County:

- Other Rural Principal Arterials
 - Serve corridor movements having trip length and travel density characteristics indicative of substantial statewide or interstate travel.
 - Connect all or nearly all urbanized areas and a large majority of Urban Clusters with population of 25,000 and over.
 - Provide an integrated network of continuous routes without stub connections (dead ends).
- Rural Minor Arterials
 - Link cities and larger towns (and other major destinations such as resorts capable of attracting travel over long distances) and form an integrated network providing interstate and inter-county service.
 - Spaced at such intervals, consistent with population density, so that all developed areas of the State are within a reasonable distance of an Arterial roadway.
 - Provide service to corridors with trip lengths and travel density greater than those predominantly served by Rural Collectors and Local Roads and with relatively high travel speeds and minimum interference to through movement.
- Rural Major Collectors
 - Provide service to any county seat not on an Arterial route, to the larger towns not directly served by the higher systems, and to other traffic generators of equivalent intra-county importance, such as consolidated schools, shipping points, county parks, important mining, and agricultural areas.
 - Link these places with nearby larger towns or cities, or with Arterial routes.
 - Serve the more important intra-county travel corridors.
- Rural Minor Collectors
 - Spaced at intervals, consistent with population density, to collect traffic from Local Roads and bring all developed areas within a reasonable distance of a Collector.
 - Provide service to smaller communities not served by a higher-class facility.
 - Link the locally important traffic generators with their rural hinterlands.
- Rural Local Roads
 - Serve primarily to provide access to adjacent land.
 - Provide service to travel over short distances as compared to higher classification categories.
 - Constitute the mileage not classified as part of the Arterial and Collector systems.

As shown in Figure 5, many of the Federal and County functional classifications of County roads align, but discrepancies between the two include the following:

- The eastern portion of Sixes River Road is classified by FHWA as a Major Collector and by the County as a Rural Minor Collector.
- Grassy Knob Road, China Mountain Road, Old Coast Road, and Grizzly Mountain Road are classified by the County as Rural Minor Collectors, but they are not Federally classified as a higher-order road.
- A section of Elk River Road is classified by FHWA as a Minor Collector and by the County as a Rural Major Collector. The extents of the Federal higher-order classification exceed those of the County classification.
- Vista Drive and Cemetery Loop Road in Port Orford are classified by the County as Rural Minor Collectors, but they are not Federally classified as higher-order roads.
- Lobster Creek Road and Road 3680 are not classified by the County as higher-order roads, but they are Federally classified as Minor Collectors.
- Agness Road is not classified by the County as a higher-order road, but it is Federally classified as a Minor Arterial (to the west) and a Major Collector (to the north).
- Rainbow Rock Road, Parkview Drive, Old County Road, W Hoffeldt Lane, E Benham Lane, and Pedrioli Drive in Brookings are classified by the County as Rural Minor Collectors, but they are not Federally classified as higher-order roads.
- The eastern section of N Bank Chetco River Road is classified by FHWA as a Major Collector and by the County as a Rural Minor Collector.

Attachment A tabulates the Federal and County functional classification of all County roads. As stated previously, the State does not provide classifications for non-State facilities. However, Table 6 provides a comparison between the Federal, State, and County functional classifications of all State facilities. As shown, there are no discrepancies between classifications according to how the agencies define them.

Table 6. State Route Functional Classification Comparison

Route #	Extents	Federal Classification	OHP Classification	County Classification
US 101	Port Orford to Brookings	Rural Other Principal Arterial	Statewide Highway	Principal Arterial
US 250	Cape Blanco to US 101	Rural Major Collector	District Highway	Major Collector
OR 255	Pistol River Road to Brookings	Rural Major Collector	District Highway	Major Collector
OR 251	Port Orford Heads State Park to US 101	Rural Major Collector	District Highway	Major Collector

Freight Routes

Curry County's transportation system includes Motor Carrier Transportation Division (MCTD) freight routes, OHP Reduction Review Routes, and the National Highway System (NHS).

Motor Carrier Transportation Division (MCTD) Designated Freight Routes

The MCTD is a division of ODOT that promotes a safe, efficient, and responsible commercial transportation industry by simplifying compliance, reducing regulatory requirements, wherever appropriate, preserving the infrastructure, enhancing the private/public partnership, fostering effective two-way communication, and delivering superior customer service while recognizing the vital economic interests of the commercial transportation industry. The MCTD provides information on over-dimension permit requirements and freight restrictions of State highways. Table 7 presents the freight restrictions relevant to the State highways in Curry County.

Table 7. Over-Dimension Vehicle Restrictions

Restriction	State Highway		
	US 101	OR 250	OR 255
Maximum Allowable Lengths ¹	Group 1 Truck-Tractor and Semitrailer – 60' Overall, 53' Trailer	Group 2 Truck-Tractor and Semitrailer – 60' Overall, 40' Trailer	Group 2 Truck-Tractor and Semitrailer – 60' Overall, 40' Trailer
Vehicles with Widths Exceeding 12 Feet	Approved for Continuous Operation	Not Approved for Continuous Operation	Not approved for Continuous Operation
Loads Exceeding 14 Feet in Height (up to 14.5 Feet)	Authorized	Not Authorized	Not Authorized
Triples Combinations	Not Authorized	Not Authorized	Not Authorized
Continuous Movement of 14 Feet Wide Mobile Homes and Modular Building Units	Authorized	Not Authorized	Not Authorized
Overall Length Limits	Pickup Truck and Trailer: 70' overall; 53' trailer	See Maximum Allowable Lengths	See Maximum Allowable Lengths
Weight-Restricted Bridges ²	Reinhart Creek Bridge Connector Road Bridge	N/A	N/A
Loads not Wider than 14 Feet ³	Authorized	Not Authorized	Not Authorized

¹Except when operating under authority of special permit. Longer lengths permitted for larger trucks. See [Route Map 1](#) for full restrictions and permit weight tables.

²See [Route Map 8](#) for weight restrictions.

³If gross weight does not exceed 98,000 pounds.

ODOT Designated Freight Routes

Although US 101 is the major roadway through Curry County for regional freight movement, the OHP does not designate it as a freight route, but it does designate it as a reduction review route. Reduction Review Routes include all parts of the State highway(s) that must be travelled to complete a prescribed route and/or connect with other State highways, including couplets and on and off ramps. The Oregon Transportation Commission may not permanently reduce the vehicle-carrying capacity of an identified freight route when altering, relocating, changing, or realigning a State highway unless safety or access considerations require the reduction.

National Highway System (NHS) Designated Freight Routes

The NHS consists of roadways important to the nation's economy, defense, and mobility, including the Interstate Highway System, Other Principal Arterials, highways that are part of the Strategic Highway Network (STRAHNET), Major Strategic Highway Network Connectors, and Intermodal Connectors. Although US 101 is an NHS route in Curry County, it is not designated as a National Highway Freight Route or State freight route on the National Network, except for the portion north of Port Orford.

Roadway Improvement Standards

Roadway improvement standards are tied to the roadway functional classification hierarchy so that the way the transportation system looks, feels, and operates is consistent across road classes. This section compiles roadway improvements standards for County roads and ODOT facilities within Curry County.

County Roads

The County's rural roadway improvement standards are summarized in Table 8. Further, County urban standards for roads within their respective UGB are summarized in Table 9. The County does not currently require a minimum paved shoulder width for its roadways. Potential modifications to the County's road standards will be reviewed as part of the alternatives analysis for the TSP update and in coordination with incorporated cities.

Table 8. County Rural Road Standards

Functional Classification (FC)	Right-of-Way (Feet) ¹	Surface Type ²	Minimum Pavement Width (Feet)	Minimum Shoulder Width (Feet) ⁴	Maximum Grade (%)
Minor Arterial	50	HMAC	26	6	10
Major Collector	50	HMAC	26	4	12
Minor Collector	50	HMAC	24	2	15
Resource/Industrial/Commercial	50	HMAC	24	2	18
Residential (11+ Dwelling Units)	50	AWDS	20 ³	2	20
Residential (5-10 Dwelling Units)	50	AWDS	18 ³	0	20
Residential (4- Dwelling Units)	50	AWDS	16 ³	0	20
Driveway	N/A	AWDS	16 ³	0	20
Turnarounds	N/A	Paved per FC	20	N/A	4

¹ Lesser width not less than 40 feet is authorized when (a) specially permitted by the County Board of Commissioners and (b) the road meets the other standards in this table. A wider than 50-foot right-of-way may be required, depending on variations or other engineering considerations.

²HMAC: Hot Mix Asphalt Concrete; AWDS: All Weather Drivable Surface

³ Inter-visible opposing turnouts required (at least one every 500 feet and where curves prohibit visibility).

⁴ Gravel shoulder outside of roadway pavement.

Table 9. County Urban Road Standards

Functional Classification	Minimum Right-of-Way (Feet)	Minimum Roadway Paved Surface Width (Feet)	Sidewalk Improvements (Feet) ¹
Brookings UGB			
Arterial/Highway	80	70	5 (both sides)
Major Collector	50	36	6 (both sides)
Hillside Street	50	24	4 (paved shoulders)
Local Road (Serving 21+ Dwelling Units)	50	36	6 (both sides)
Local Road	45	30	6 (both sides)
Commercial Industrial Road	60	44	6 (both sides)
Alley	20	20	None
Cul-de-sac	45R	36R	6
Gold Beach UGB			
US 101²			
<i>Section 1 (Rogue River Bridge to Moore St)</i>	80	70	5 (both sides)
<i>Section 2 (Moore St to Caughell St)</i>	80	56	6 (both sides)
<i>Section 3 (Caughell St to 4th St / 8th St to Pacific Dr)</i>	80	64	6 (both sides)
<i>Section 4 (4th St to 8th St)</i>	80	48	6 (both sides)
<i>Section 5 (South of Pacific Dr)</i>	80	34	6 (both sides)
Major Collector	50	36	5 (both sides)
Minor Collector	50	24	5 (one side)
Hillside Street	50	24	2 (gravel shoulders)
Local Road (Water/ Sewer Available)	50	30	4 (both sides) / 6 (one side)
Local Road (Water/ Sewer Not Available)	50	24	6 (striped ped/bike path one side)
Commercial Industrial Road	60	40	5 (both sides)
Alley	20	20	None
Cul-de-sac	60R	45R	5
Port Orford UGB			
Arterial/Highway	80	70	5 (both sides)
Major Collector	50	36	6 (both sides)
Minor Collector	50	24	5 (one side)
Hillside Street	50	24	2 (gravel shoulders)
Local Road (Water/ Sewer Available)	50	30	6 (both sides)
Local Road (Water/ Sewer Not Available)	50	24	6 (one side)
Commercial Industrial Road	60	40	6 (both sides)
Alley	20	20	None
Cul-de-sac	60R	50R	5

¹Different means of providing for pedestrian or bicycle transportation may be allowed if it is consistent with existing conditions on adjacent properties/developments.

²Segments of US 101 are defined in the City of Gold Beach TSP; ODOT is the authority for these areas.

State Highways

As State facilities, the following highways are generally subject to the rural typical sections described in the 2023 Highway Design Manual (HDM – Reference 5) for arterials, collectors, and local routes:

- US 101; Rural Other Principal Arterial
 - Humbug Mountain Front Road; Rural Local
 - Reinhart Creek Front Road; Rural Local
 - O'Brien Creek Frontage Road; Rural Local
 - Nesika Beach Frontage Road; Rural Minor Collector
 - Giesel Mountain Connection; Rural Minor/Major Collector
 - Wedderburn Loop Front Road; Rural Local
 - Hunter Creek Connection (No. 1); Rural Major Collector
 - Mack Arch Connection; Rural Local
 - McVay Lane; Urban Local
- OR 250; Rural Major Collector
- OR 251; Rural Major Collector
- OR 255 (Carpenterville Highway); Rural Minor/Major Collector

Applicable HDM typical sections are summarized in Table 10 for 3R projects and Table 11 for 4R projects.

The HDM also recognizes that one typical section does not fit all roadway contexts and that the State highway system may be rural through most of Curry County, but it still crosses through incorporated cities and unincorporated communities. Therefore, the HDM also offers cross-section design guidance that is a context-sensitive approach to roadway improvements by taking into account adjacent land uses and all roadway users. The HDM establishes six urban contexts to consider when identifying appropriate roadway cross sections:

- Traditional Downtown/Central Business District
- Urban Mix
- Commercial Corridor
- Residential Corridor
- Suburban Fringe
- Rural Community

These urban contexts are likely to align with the various contexts for the State highways in Curry County, particularly US 101. Attachment B tabulates the recommended cross section elements for State highways in these urban contexts.

Table 10. HDM Rural Typical Section Standards for State Arterials, Collectors, and Local Routes (3R)

Design Year Volume (ADT)	Average Running Speed	Lane Width (Feet) ¹	Shoulder Width (Feet)
Less than 750 Vehicles	All Speeds	10	2
750 to 2,000 Vehicles	Under 50 MPH	11	2
	50 MPH and Over	11	3
Over 2,000 Vehicles	All Speeds	11	4

¹A minimum 11-foot lane is required on all NHS Routes on ODOT jurisdiction roadways only. Local agencies may use American Association of State Highway and Transportation Officials (AASHTO) standards for lane width on Local Agency jurisdiction roads.

Table 11. HDM Rural Typical Section Standards for State Arterials, Collectors, and Local Routes (4R)

Design Feature	Two-Lane								Four-Lane	
	ADT ≤ 400		ADT 400-1,500		ADT 1,500-2,000		ADT ≥ 2,000		DHV > 700	
Design Speed (MPH)	50	45	55	45	55	50	55	50	55	50
Rural Arterials										
Width of Travel Way (Feet)	22	22	24	22	24	22	24	24	2 x 24	
Shoulder Width (Feet)	4	4	6	6	6	6	8	8	8	8
Max Grade (%)	5 (6)	6 (8)	4	6	4	6	4	6	4	6
Rural Collectors										
Width of Travel Way (Feet)	20	20	22	22	24	22	24	24	2 x 24	
Shoulder Width (Feet)	2	2	5	5	6	6	8	8	8	8
Max Grade (%)	6 (8)	6 (9)	6	6	5	6	5	6	5	6
Rural Local Routes										
Width of Travel Way (Feet)	20	18	22	22	24	22	24	24	2 x 24	
Shoulder Width (Feet)	2	2	5	5	6	6	8	8	8	8
Max Grade (%)	6 (8)	6 (9)	6	6	5	6	5	6	5	6

(Maximum grade recommended for ADT < 250)

Access Spacing Standards

Providing adequate access to streets, land uses, and key destinations is a critical element of operating and planning an effective transportation system for all users. Curry County and ODOT maintain access spacing standards to help balance the needs of through travelers, including freight and transit, and of area residents, employees, and visitors. Access management typically increases access spacing on higher classified roads to prioritize mobility and decreases access spacing on lower classified roads to prioritize local access.

County Roads

Curry County establishes spacing standards in its Road Standards (October 2017), which are summarized in Table 12 below. These spacing standards are for intersection to intersection and intersection to driveway and are based on roadway functional classification.

Table 12. Curry County Access Spacing Standards

Functional Classification	Public Road Type	Spacing ¹	Private Driveway Type	Spacing ²
Arterial	At-Grade	¼ mile	Left/Right Turns	500 Feet
Collector	At-Grade	250 Feet	Left/Right Turns	100 Feet
Local Road	At-Grade	250 Feet	Left/Right Turns	Each Lot
Alley	At-Grade	250 Feet	Left/Right Turns	Each Lot

¹Between roads

²Between driveways and intersections (measured from center to center)

The majority of the County's arterial and collector network generally complies with the required access spacing, except in some instances near and within the urbanized areas and in limited rural locations along the following roadways:

- Cedar Valley Drive
- N Bank Rogue River Road
- Hunter Creek Road
- Pistol River Loop Road
- Cape Ferrelo Road
- N Bank Chetco River Road
- Ocean View Drive
- Winchuck River Road

State Highways

ODOT establishes access management spacing standards in the OHP and Oregon Administrative Rule (OAR) 734-051-4020(8) to address traffic mobility and safety on the State highway system. These standards provide adequate perception and reaction time for a driver to see an object in the road and react appropriately to avoid colliding with another vehicle that is entering or exiting the highway at a downstream driveway or road intersection. The access management spacing standards applicable to the State highways within Curry County are summarized in Table 13 and are based on Annual Average Daily Traffic (AADT), posted speed limit, and street functional classification.

Much of US 101 does not meet the access management spacing standards summarized in Table 13, particularly within and surrounding the urban areas, except for stretches of highway in the rural areas between the incorporated cities and north of Port Orford. Except in some instances, OR 250, OR 251, and OR 255 generally meet their required access management spacing standards.

Table 13. ODOT Access Management Spacing Standards for State Highways in Curry County

Posted Speed (MPH)	Area Type	AADT	Extents	Access Spacing (Feet)
US 101 (Statewide Highway)				
55+	Rural	>5,000	South County Boundary to Brookings	1,320
55+	Rural	<5,000	<ul style="list-style-type: none"> • Brookings to Gold Beach • Gold Beach to Port Orford • Port Orford to Langlois (south limit) • Langlois (north limit) to North County Boundary 	1,320
40-45	Urban	>5,000	<ul style="list-style-type: none"> • Brookings (Chetco River to Pedrioli Dr; Harris Park Rd to W Harris Hts) • Port Orford (between Cemetery Loop Rd) • Gold Beach (Jerry's Flat Rd to Wedderburn Loop Rd; Pacific Vista Dr to Weber Way) 	800
40-45	Urban	<5,000	Port Orford (between Cemetery Loop Rd; Madrona Ave to Paradise Point Rd)	360
40-45	Unincorporated Community	<5,000	Langlois	750
30-35	Urban	>5,000	<ul style="list-style-type: none"> • Port Orford (13th St to 19th St) • Gold Beach (Weber Way to Jerry's Flat Rd) • Brookings (Chetco River to eastern Downtown limit; western Downtown limit to Harris Park Rd) 	500
30-35	Urban	<5,000	Port Orford (Cemetery Loop Rd to 13th St; 19th St to Madrona Ave)	250
25 & Lower	Urban	>5,000	Downtown Brookings	350
OR 250 (District Highway)				
55+	Rural	<5,000	US 101 to MP 3.15	650
OR 251 (District Highway)				
25 & Lower	Rural	<5,000	US 101 to MP 0.0	150
OR 255 (District Highway)				
55+	Rural	<5,000	US 101 to US 101 (north loop); Demoss Rd to US 101 (south loop)	650
40-45	Urban	<5,000	US 101 to Demoss Rd (south loop)	360

Roadway Characteristics

This section summarizes the characteristics of roadways within Curry County, including their pavement type, conditions, and widths; available right-of-way; presence of medians; any on-street parking locations; posted speed limits; and stop-control devices.

Pavement Type and Condition

Roadways in the county include three surface type categories: asphalt concrete (AC), oil mat, and gravel. As stated previously, these surface types make up the following percentages of County roads:

- Asphalt Concrete (AC): 85%
- Oil Mat: 4%
- Gravel: 11%

These roadway types and their locations can be seen in Figure 6. As shown, the majority of major roads, including US 101 and those branching off US 101, are paved. There are a number of paved roads that transition to gravel roads, such as Old County Road, Hunter Creek Road, China Mountain Road, and Floras Creek Road. A limited number of short oil mat roads are provided throughout the County, mostly along US 101.

County Roadway Pavement Conditions

The pavement conditions of County roadways are illustrated in Figure 7. As shown, most County highways are in fair condition or better. Hensley Hill Road has a “Poor” rating for its entire extent; several small sections of roadway in Brookings, Gold Beach, and Port Orford have a “Poor” or “Very Poor” rating as well.

State Highway Pavement Conditions

ODOT conducts pavement condition surveys biennially during even-numbered years. Therefore, the conditions summarized in this section are based on ODOT’s 2020 Pavement Condition Report. ODOT employs two separate and distinct pavement rating procedures: the Automated Distress Survey procedure and the Good-Fair-Poor (GFP) rating procedure.

- The Automated Distress Survey is used to determine specific pavement problems and deficiencies, and therefore, is costly and time and labor intensive, so it is applied to Interstates, NHS routes, and selected higher volume non-NHS routes. US 101 is an NHS route in Curry County.
- The GFP rating procedure is applied to remaining routes and uses a subjective Good-Fair-Poor rating method, which relies on visual inspection of pavement surface and is rated from 0 to 100 based on the ride quality and surface distresses. The indexes resulting from both methodologies are then categorized into five conditions: “Very Good”, “Good”, “Fair”, “Poor” and “Very Poor.”

As shown in Figure 7, most State highways are in fair condition or better. The following sections have a “Poor” or “Very Poor” condition:

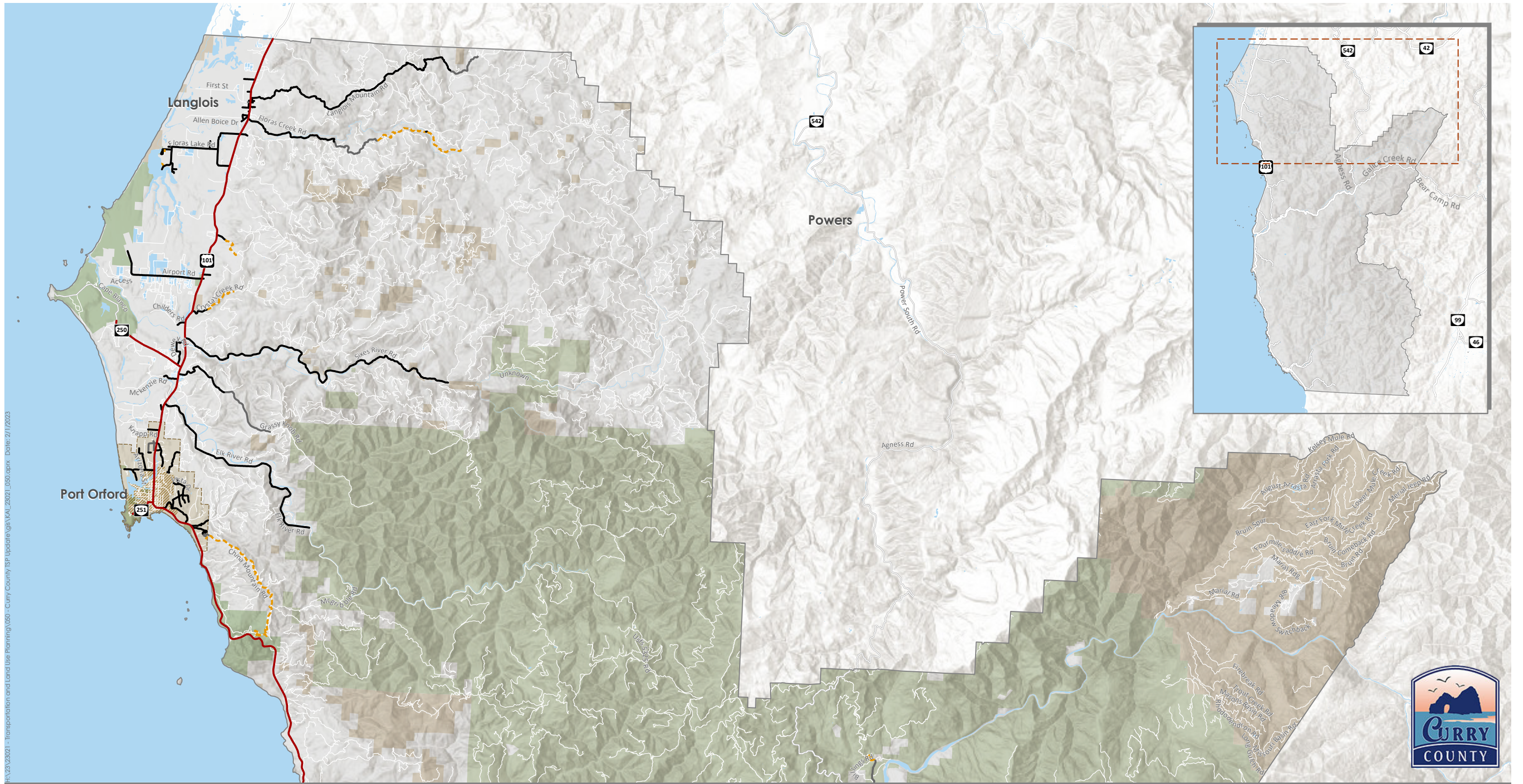
- OR 251 for entire extents: Poor
- US 101 from MP 327.88 to MP 330.00 (Gold Beach): Poor
- OR 255 from MP 334.87 to MP 339.68: Very Poor
- US 101 from MP 356.18 to MP 357.87 (Brookings): Poor

Travel Lanes and Shoulder Widths

Figure 8 illustrates the number of lanes on State and County roadways. Most State and County facilities are two-lane roadways, including US 101. Along US 101, there are several three- and four-lane sections. The four-lane sections through Brookings and Gold Beach generally are comprised of two travel lanes in each direction and sometimes include a center turn lane.

In addition, the section of US 101 near OR 250 consists of two travel lanes in each direction and a left-turn lane onto OR 250. The other three- and four-lane sections between urban areas have stretches of either one or two passing lanes.

Pullouts are also available throughout the US 101 corridor, especially between Gold Beach and Port Orford. Many of these are for recreational use, while others are primarily for emergency use and turnarounds.



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- State Paved Roads
- County Paved Roads
- - - County Gravel Roads
- - - County Oil Mat Roads



Figure 6

**Pavement Types
Curry County, Oregon**

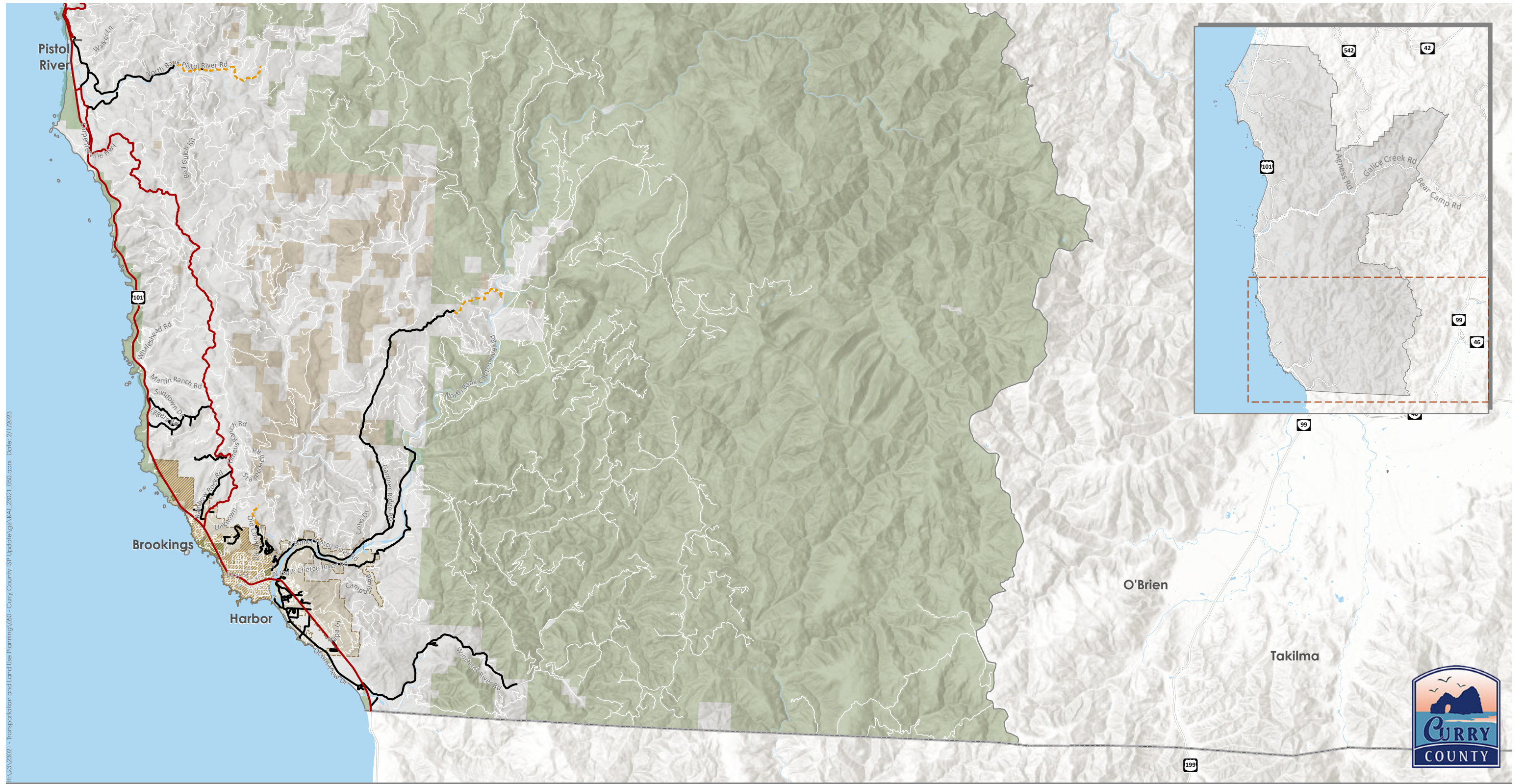


- State Paved Roads
- County Paved Roads
- County Gravel Roads
- County Oil Mat Roads



Figure 6

**Pavement Types
Curry County, Oregon**



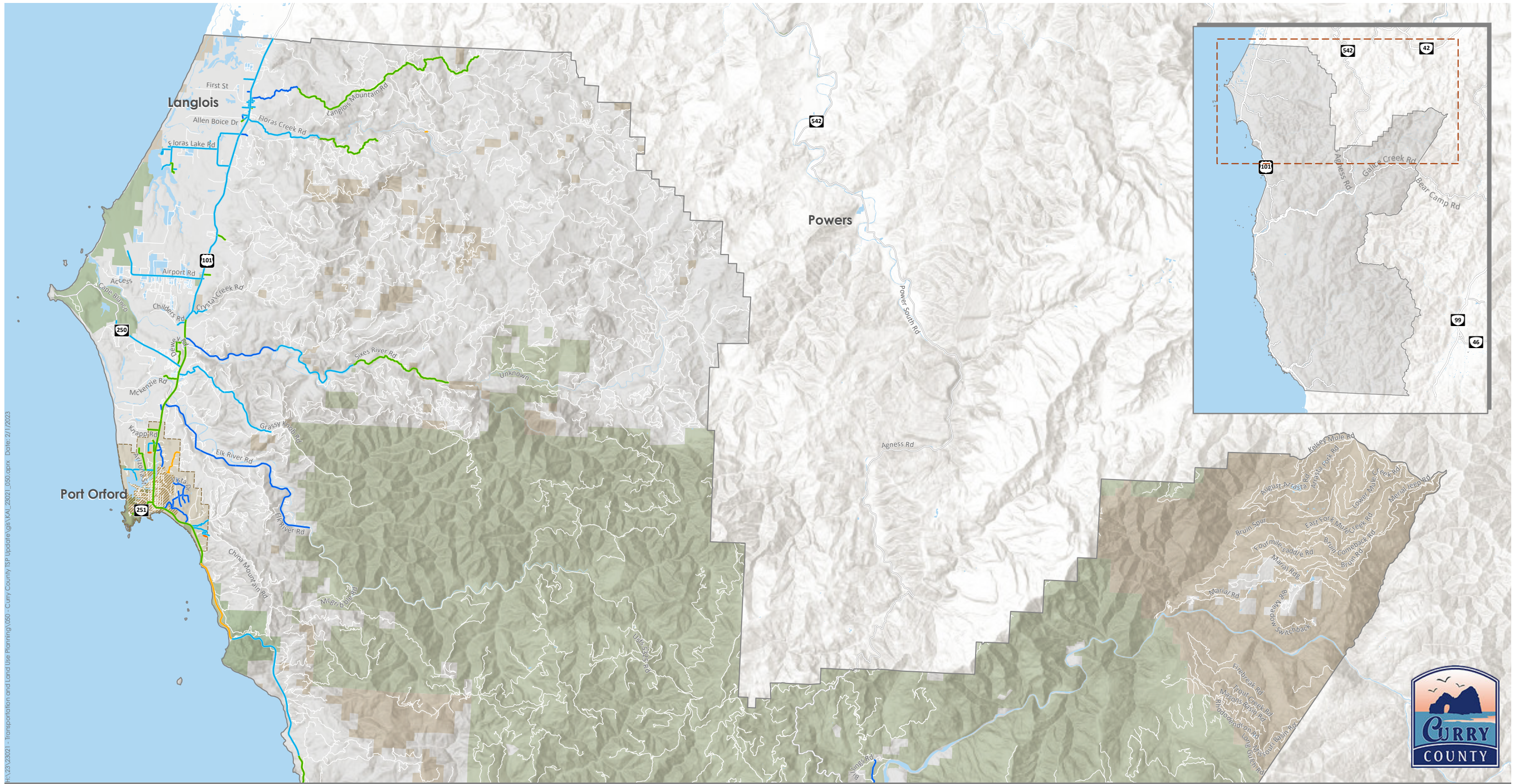
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- State Paved Roads
- County Paved Roads
- County Gravel Roads
- County Oil Mat Roads



Figure 6

**Pavement Types
Curry County, Oregon**



Pavement Conditions

- Very Good
- Good
- Fair
- Poor
- Very Poor



Figure 7

**Pavement Conditions
Curry County, Oregon**

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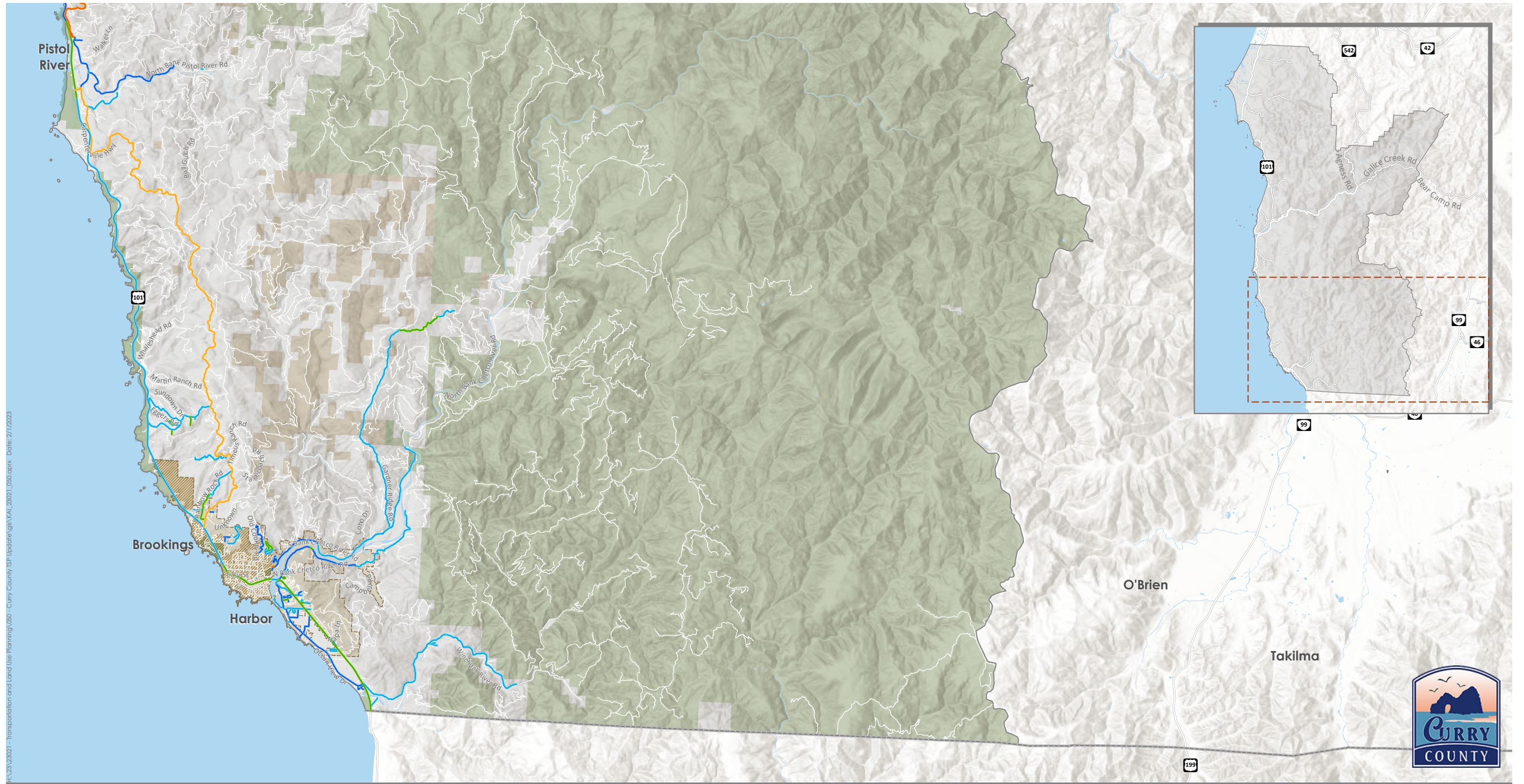


- Pavement Conditions**
- Very Good
 - Good
 - Fair
 - Poor
 - Very Poor



Figure 7

**Pavement Conditions
Curry County, Oregon**



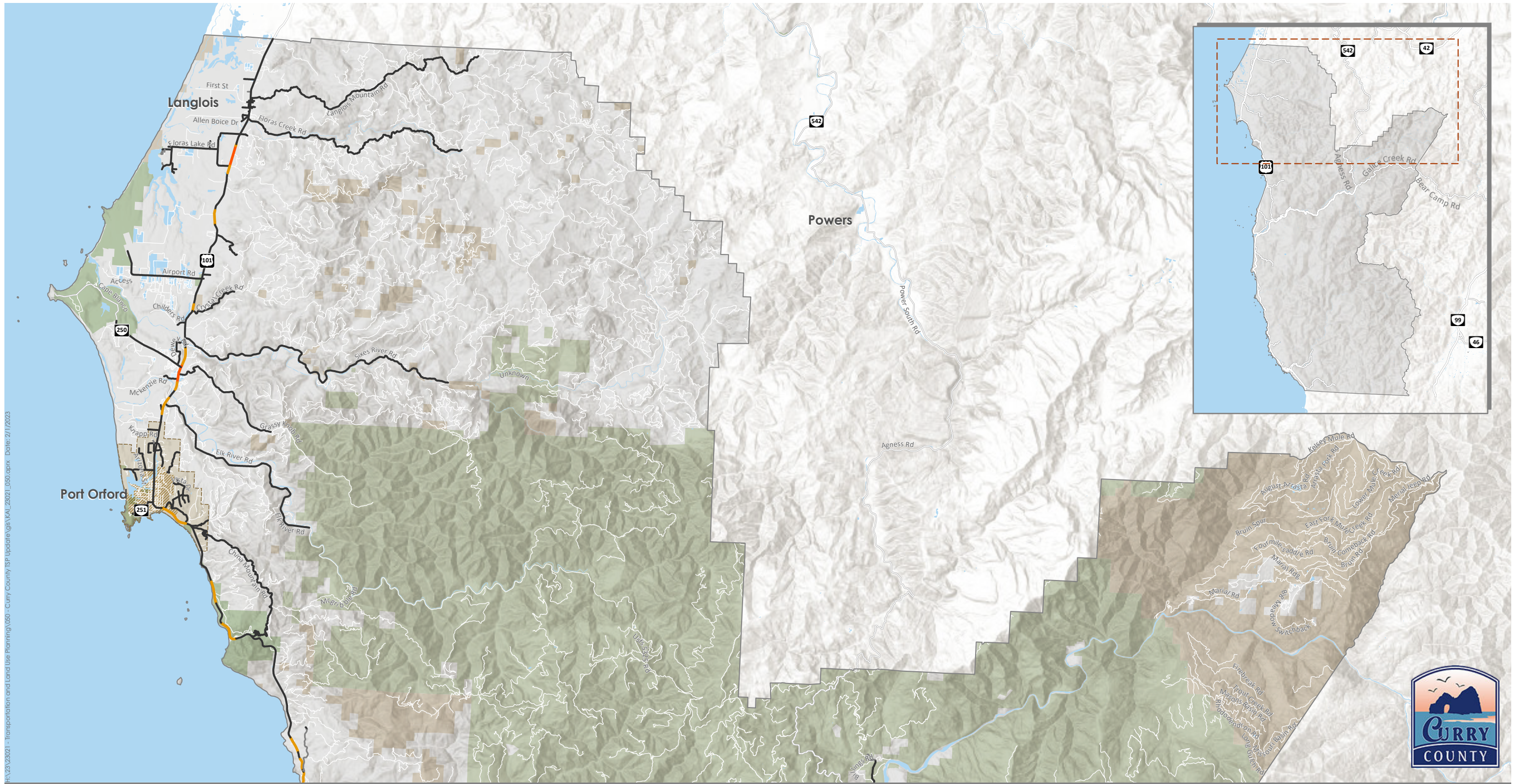
FILE:2/23/2021 - Transportation and Land Use Planning/060 - Curry County TSP Update/fig/VOL_23021_060.aprx Date: 2/17/2023

- Pavement Conditions**
- Very Good
 - Good
 - Fair
 - Poor
 - Very Poor



Figure 7

**Pavement Conditions
Curry County, Oregon**



- 1 Lane
- 2 Lanes
- 3 Lanes
- 4 Lanes



Figure 8

**Travel Lanes
Curry County, Oregon**

FILE: 2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/fig8_VA_23021_060.aprx Date: 2/17/2023

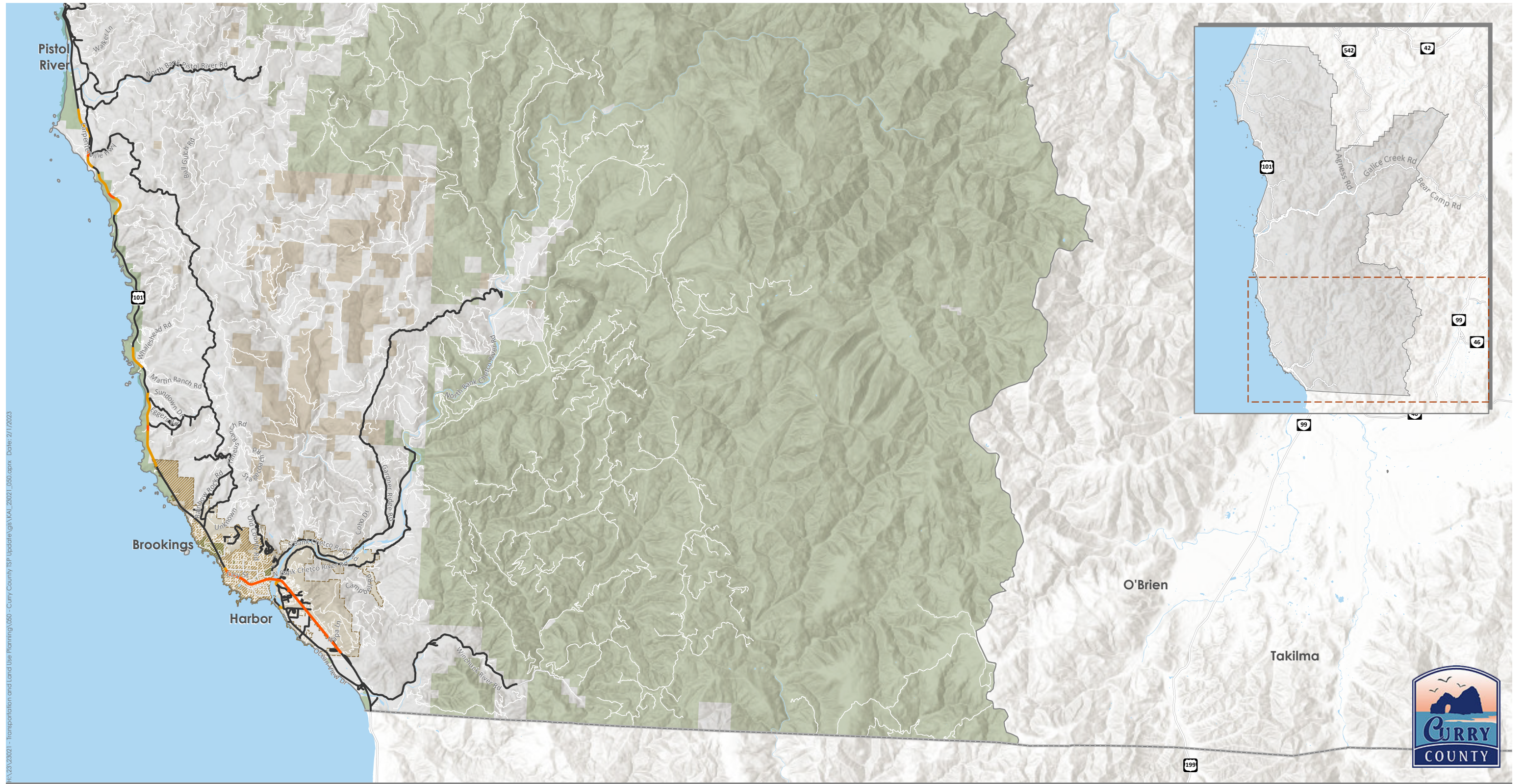


- 1 Lane
- 2 Lanes
- 3 Lanes
- 4 Lanes



Figure 8

**Travel Lanes
Curry County, Oregon**



FILE:2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/fig8_VA_23021_060.aprx Date: 2/17/2023

- 1 Lane
- 2 Lanes
- 3 Lanes
- 4 Lanes



Figure 8

**Travel Lanes
Curry County, Oregon**

Figure 9 illustrates shoulder widths in Curry County. Along US 101, shoulder widths vary from two feet to 11 feet, with the majority of sections having paved shoulder of greater than four feet. Most County facilities have no paved shoulder at all.

- 8% of State roadways and 96% of County roadways have paved shoulder widths of 2 feet or narrower.
- 55% of State roadways and 1% of County roadways have paved shoulder widths of 6 feet and wider.

US 101 has mostly Asphalt Concrete shoulders, with some stretches also having Portland Cement Concrete. Where paved shoulders are available along County arterials and collectors, they are generally Asphalt Concrete.

Right-of-way

Right-of-way refers to the overall width of roadway jurisdiction that typically expands beyond the physical roadway section and provides space for future roadway improvements, such as roadway widening, added bike or pedestrian facilities, etc. According to Table 8 in previous sections, the County requires 50 feet of right-of-way for all rural roads. Forty feet of right-of-way can be authorized when (a) specially permitted by the County Board of Commissioners and (b) the road meets the other standards. A wider than 50-foot right-of-way may be required, depending on variations or other engineering considerations. Right-of-way required for urban County roads varies depending on location and functional classification, as summarized previously in Table 9.

Existing right-of-way widths for State and County facilities are not available at this time.

Medians

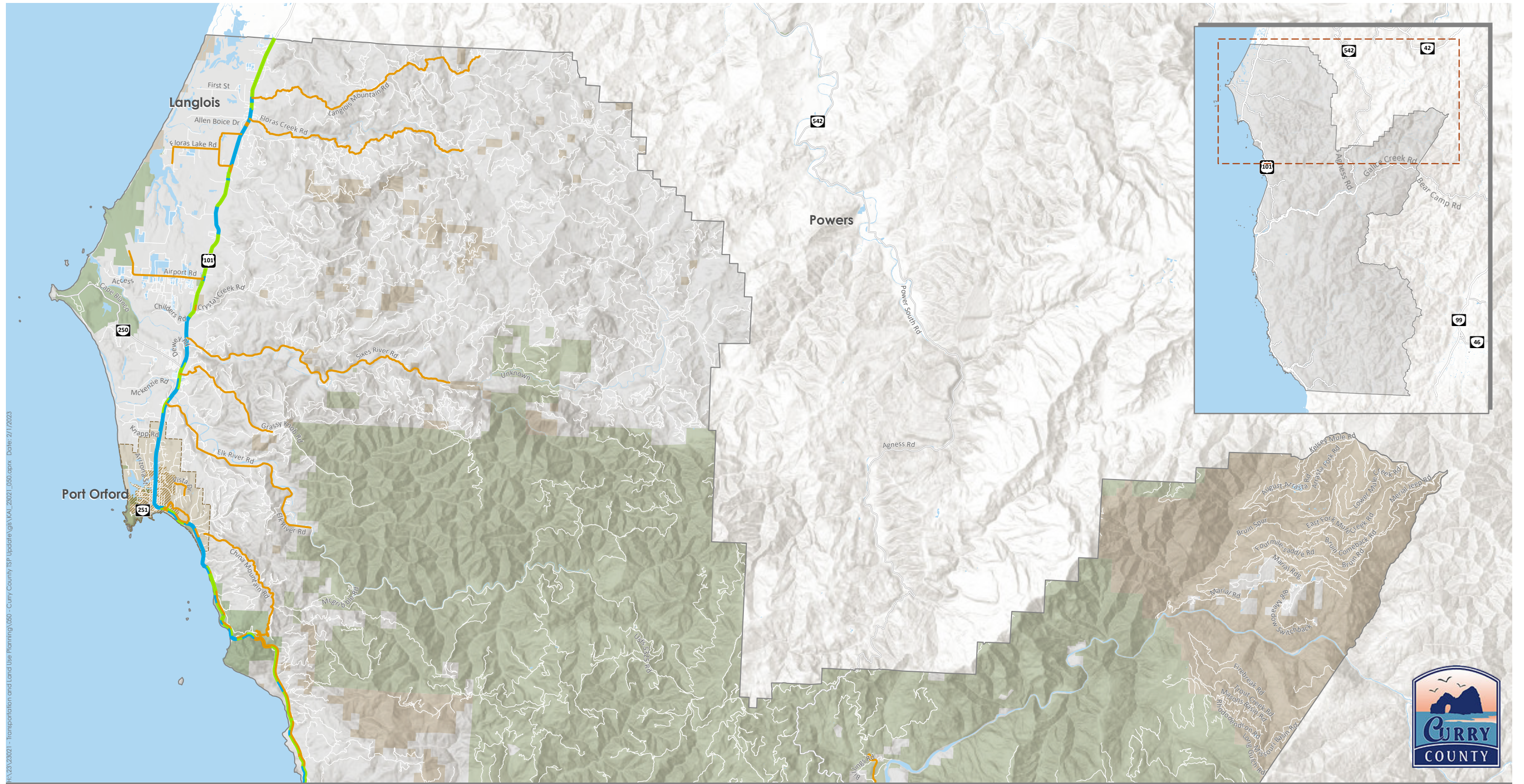
Figure 10 presents a map of medians on State and County roadways. Known median treatments on arterials and collectors in the county include the following:

- A concrete median on N Bank Chetco Road at the intersection with US 101
- Painted medians on US 101
 - Near major intersections
 - North of Seahorse Lane
 - In the Brookings area from Raymond Lane to Oak Street and Pacific Avenue to Arnold Lane (primarily center turn lanes)
 - North of Hunter Creek Road
 - In the Gold Beach area from Pacific Vista Drive to Shirley Lane and Gauntlett Street to Jerry's Flat Road (center turn lane)
 - In the Port Orford area from Cemetery Loop Road to Paradise Point Road

On-street Parking Locations

On-street parking is generally not provided along State or County facilities within the unincorporated areas of Curry County. On-street parking along State facilities is limited to a few streets within the incorporated cities.

GIS information on on-street parking locations is not available at this time.



- Paved Shoulders Less than 4 Feet Wide
- Paved Shoulders 4 Feet to Less than 6 Feet Wide
- Paved Shoulders 6 Feet or Wider



Figure 9

**Paved Shoulder Widths
Curry County, Oregon**

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- Paved Shoulders Less than 4 Feet Wide
- Paved Shoulders 4 Feet to Less than 6 Feet Wide
- Paved Shoulders 6 Feet or Wider



Figure 9

**Paved Shoulder Widths
Curry County, Oregon**

FHWA 2023/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/ghk/VAL_23021_060.oprx Date: 2/1/2023



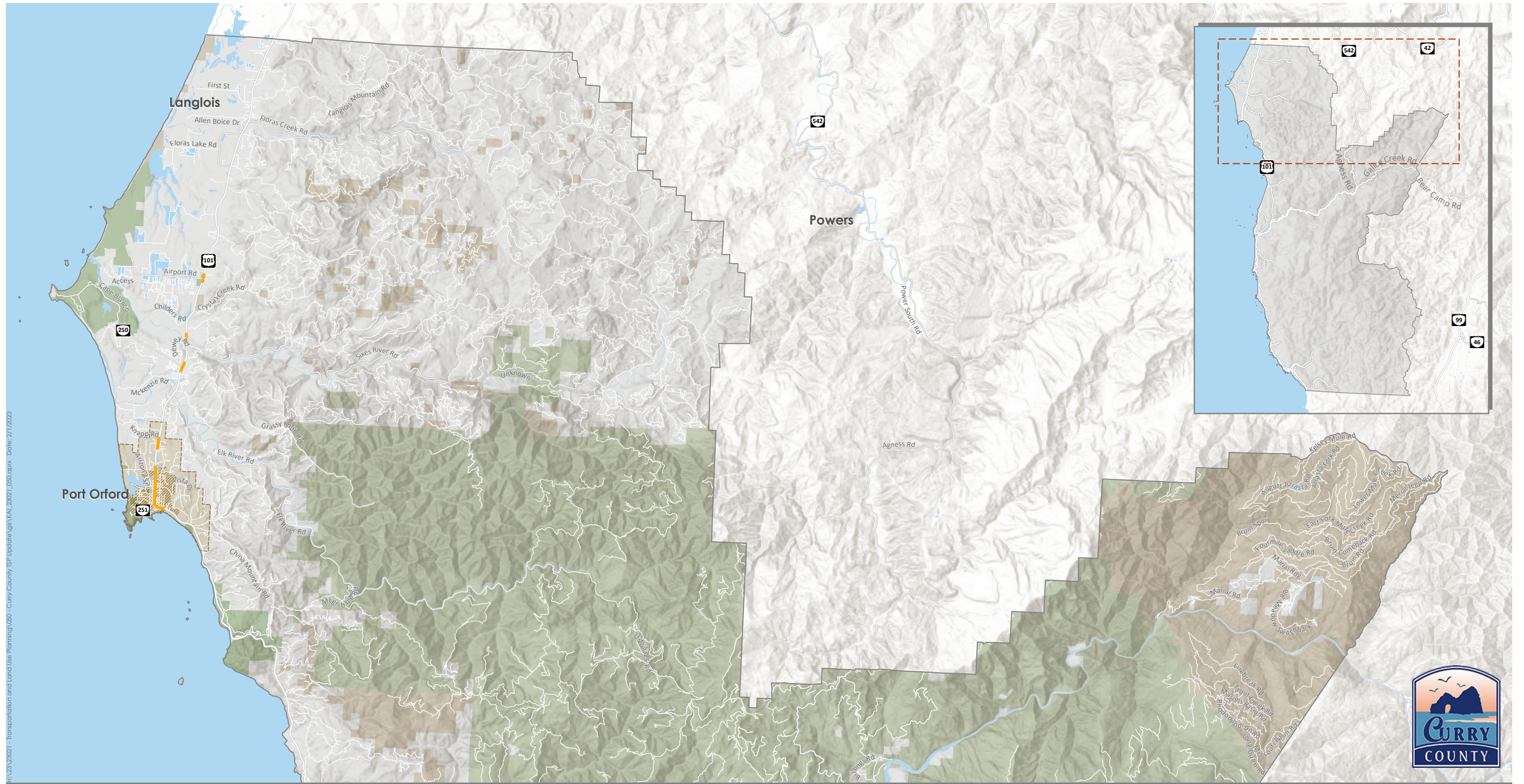
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- Paved Shoulders Less than 4 Feet Wide
- Paved Shoulders 4 Feet to Less than 6 Feet Wide
- Paved Shoulders 6 Feet or Wider



Figure 9

**Paved Shoulder Widths
Curry County, Oregon**





-  Curbed Median
-  Painted Median



Figure 10

Medians
Curry County, Oregon



FHV 2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update vglx\VAI_23021_060.aprx Date: 2/1/2023

- Curbed Median
- Painted Median



Figure 10

**Medians
Curry County, Oregon**



FILE:2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/fig10_VA1_23021_060.aprx Date: 2/17/2023

- Curbed Median
- Painted Median



Figure 10

Medians
Curry County, Oregon

Posted Speed

The posted speed limit of a roadway typically corresponds with its functional classification, with higher classifications (e.g., arterials and collectors) having greater speeds and lower classifications (e.g., local roads) having lower speeds. Exceptions to this correlation are for rural facilities with lower classifications having high speeds and vice versa.

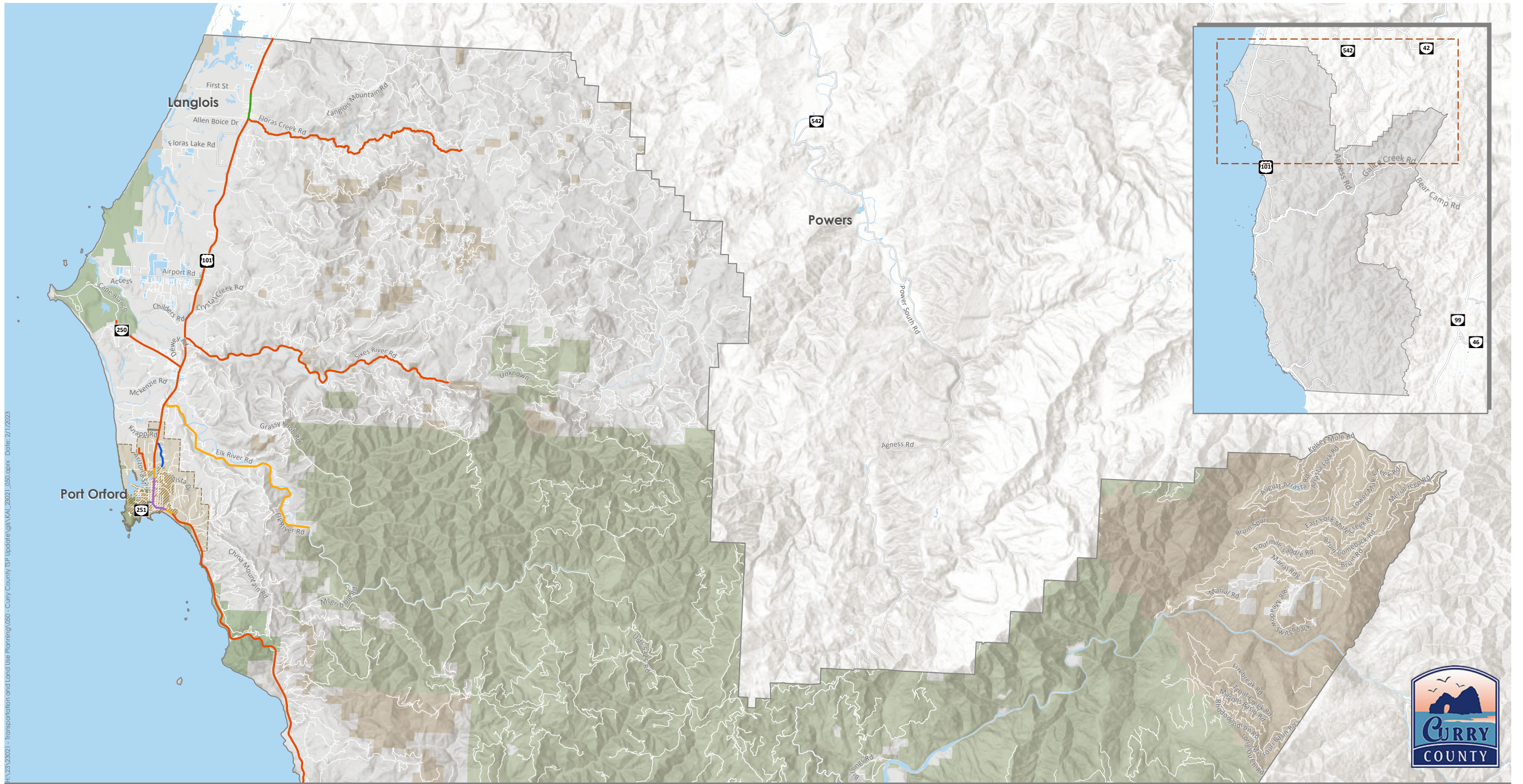
Figure 11 shows the posted speed limits on roadways within the study area. As shown, the speed limit on US 101 is generally 55 MPH, with reduced speed sections as low as 25 MPH through major urban areas, such as Port Orford, Gold Beach, and Brookings. Most major County facilities have speed limits of 45 or 55 MPH. Sections of N Bank Rogue River Road, N Bank Chetco River Road, S Bank Chetco River Road, and Oceanview Drive have speed limits of 40 MPH or less.

Intersection Traffic Control Devices

The eight intersections studied for the TSP that are shown in Figure 12 are all two-way stop-controlled. Beyond these, the majority of public street intersections in the county are stop sign controlled or uncontrolled. The only known signalized intersections in Curry County are along US 101 in Brookings and north of Gold Beach.

Roadway Network Findings

- Multiple jurisdictions own roadways across Curry County, including the County, ODOT, ODF, USFS, BLM, and the incorporated cities. The USFS owns 37 percent of all known roads in the region (most within the Rogue River-Siskiyou National Forest).
- The county generally lacks major east-west roadway connections between US 101 and the State highway system east of Curry County (e.g., I-5). USFS roads that might be considered for use as either connections to the State highway system or as alternative routes include Hunter Creek Road, Agness Road, Galice Creek Road, Sawtooth Forest Road, and Elk River Road.
- There are discrepancies in Federal and County functional classifications of Sixes River Road, Grassy Knob Road, China Mountain Road, Old Coast Road, Grizzly Mountain Road, Elk River Road, Vista Drive, Cemetery Loop Road, Lobster Creek Road, Road 3680, Agness Road, Rainbow Rock Road, Parkview Drive, Old County Road, W Hoffeldt Lane, E Benham Lane, Pedrioli Drive, and N Bank Chetco River Road.
- US 101, OR 250, and OR 255 have length and width restrictions for moving freight and are not authorized to move triples combinations. US 101 includes 2 weight restricted bridges. OR 250 and OR 255 are not authorized to continuously move 14 feet wide mobile homes / modular building units.
- None of the State highways are designated by the OHP as freight a route, but the OHP designates US 101 as a Reduction Review Route. US 101 is also an NHS route but is not designated as a National Highway Freight Route or State freight route on the National Network, except for the portion north of Port Orford.
- The County's rural roadway improvement standards do not specify a minimum width for paved shoulders; in turn, much of the County's rural roadway network lacks paved shoulders and many roadways are substandard.



- 55 MPH
- 45 MPH
- 40 MPH
- 35 MPH
- 30 MPH
- 25 MPH



Figure 11

**Posted Speed Limits
Curry County, Oregon**

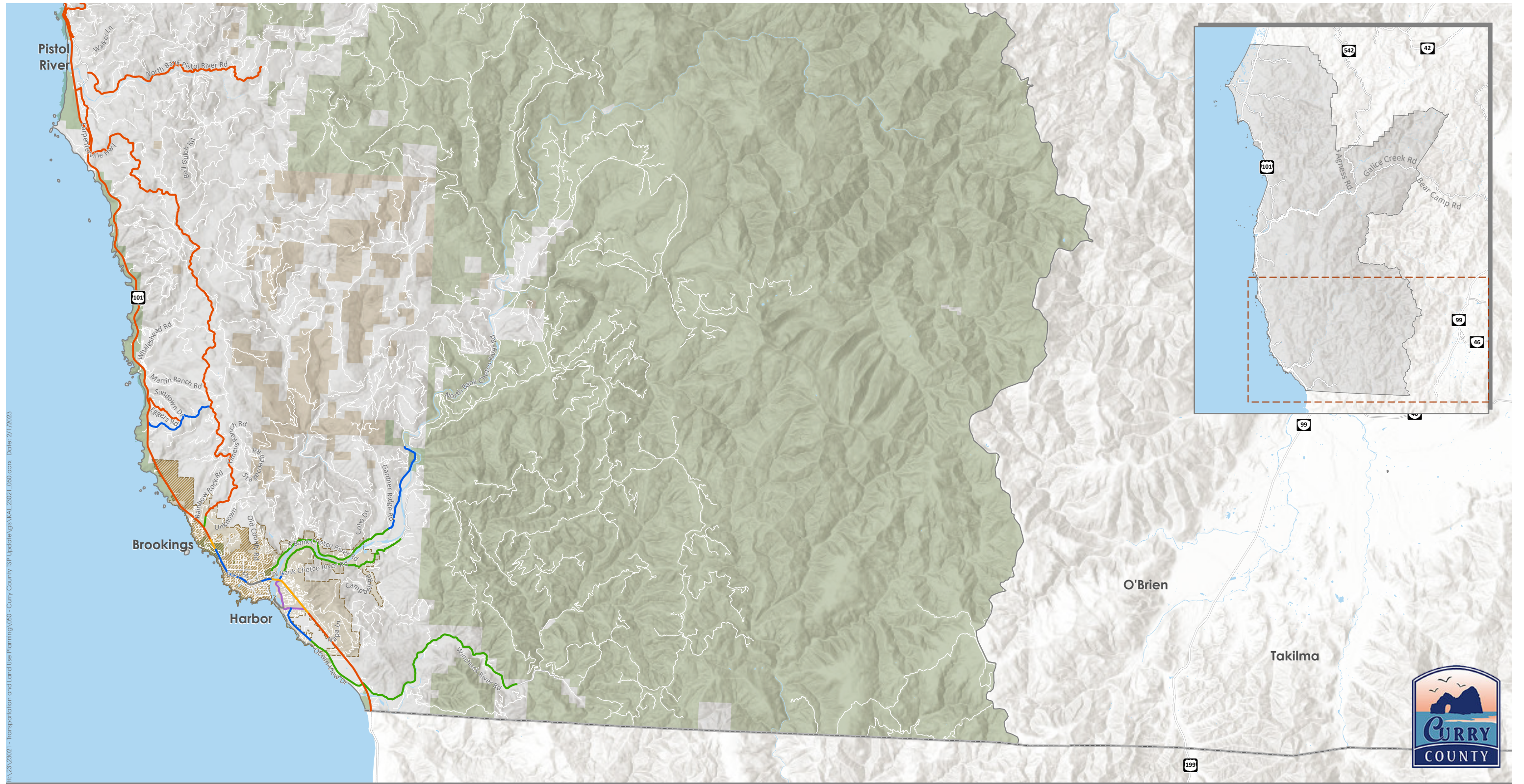


- 55 MPH
- 45 MPH
- 40 MPH
- 35 MPH
- 30 MPH
- 25 MPH



Figure 11

**Posted Speed Limits
Curry County, Oregon**



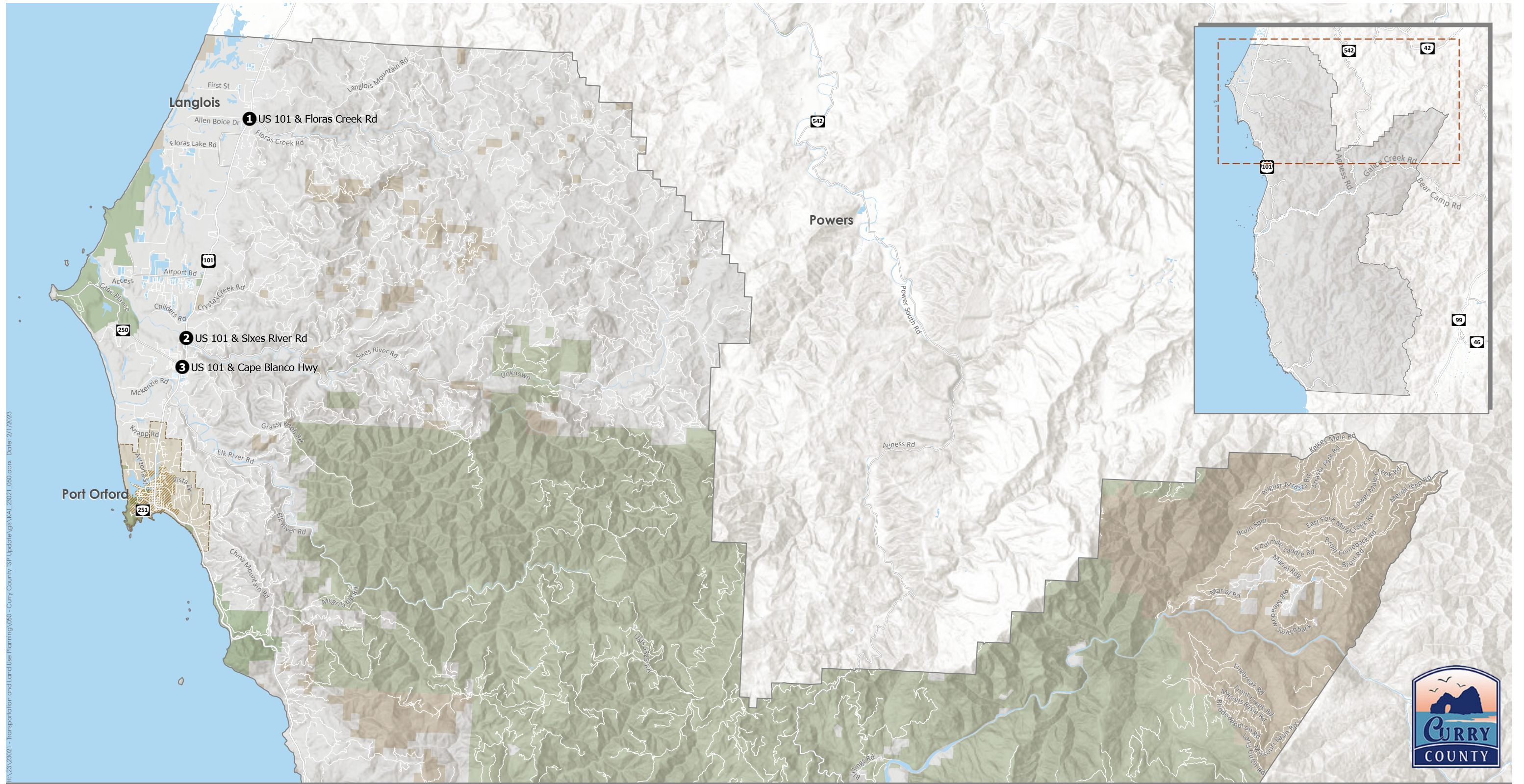
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- 55 MPH
- 45 MPH
- 40 MPH
- 35 MPH
- 30 MPH
- 25 MPH



Figure 11

**Posted Speed Limits
Curry County, Oregon**



FILE:2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/fig12_VA_23021_060.oprx Date: 2/17/2023

● Study Intersections



Figure 12

**Study Intersections
Curry County, Oregon**



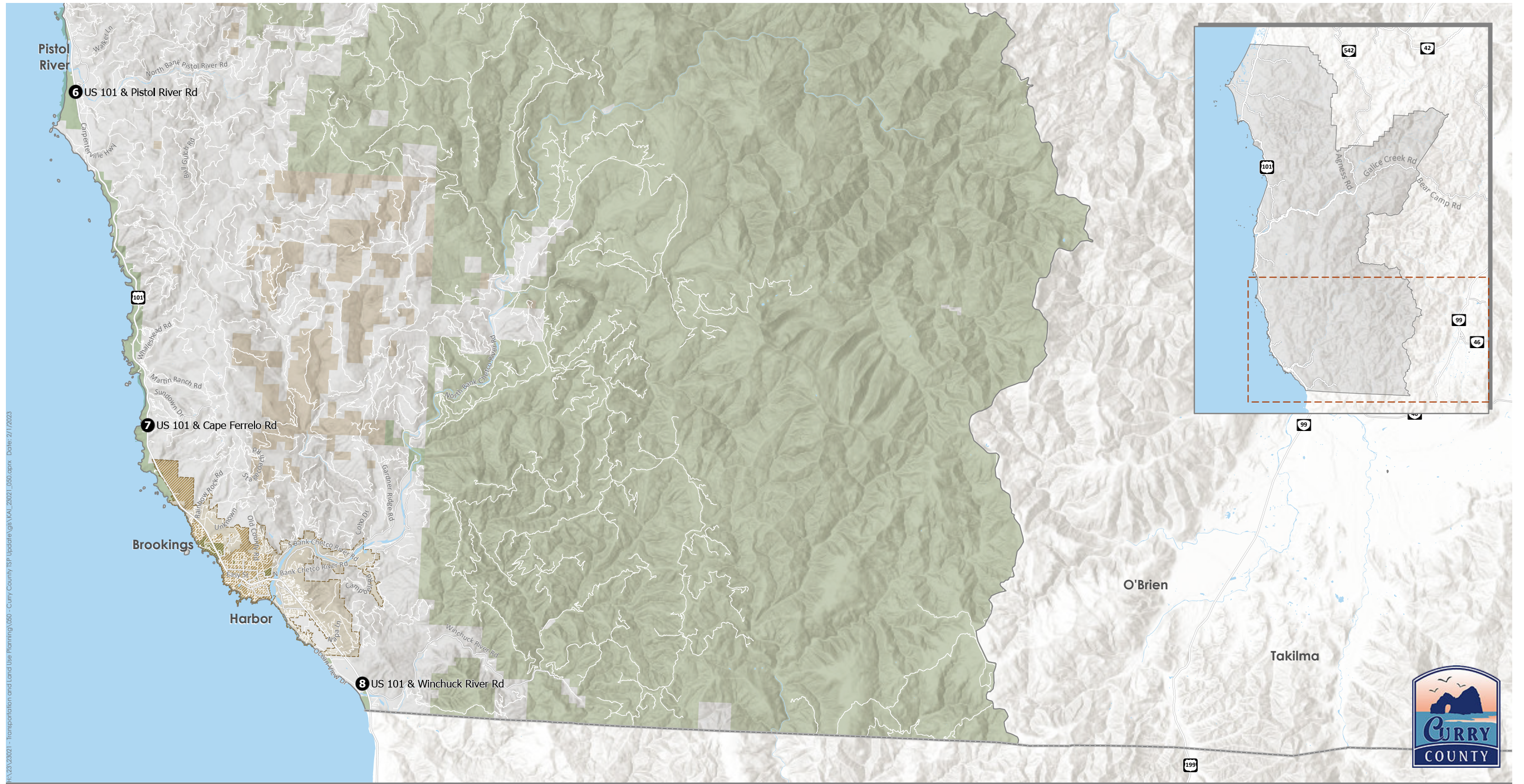
FHV 2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/ghk/VAL_23021_060.oprx Date: 2/1/2023

● Study Intersections



Figure 12

**Study Intersections
Curry County, Oregon**



FILE:2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/fig12_VA1_23021_060.aprx Date: 2/17/2023

● Study Intersections



Figure 12

**Study Intersections
Curry County, Oregon**

Roadway Network Findings (Continued)

- Most of the County's arterial and collector network generally complies with its required access spacing, except along some instances of Cedar Valley Drive, N Bank Rogue River Road, Hunter Creek Road, Pistol River Loop Road, Cape Ferrelo Road, N Bank Chetco River Road, Ocean View Drive, Winchuck River Road.
- Most County and State highways have fair pavement condition or better. Hensley Hill Road, OR 251, and sections of US 101 near Gold Beach and Brookings have "Poor" ratings; several small sections of roadway in Brookings, Gold Beach, and Port Orford have "Poor" or "Very Poor" ratings; and a section of OR 255 has a "Very Poor" rating.
- Most State and County facilities are two-lane roadways, including US 101, except in and around Brookings and Gold Beach. US 101 includes several sections of passing lanes throughout the County, as well as pull-outs.
 - 8% of State roadways and 96% of County roadways have paved shoulder widths of 2 feet or narrower. 55% of State roadways and 1% of County roadways have paved shoulder widths of 6 feet and wider.
 - Some medians treatments are provided along US 101 that are primarily painted, as opposed to concrete.
- The speed limit on US 101 is generally 55 MPH, except through major urban areas. Most major County facilities have speed limits of 45 or 55 MPH, but sections of N Bank Rogue River Road, N Bank Chetco River Road, S Bank Chetco River Road, and Oceanview Drive have speed limits of 40 MPH or less.
- Most public street intersections in the county are stop sign controlled or uncontrolled. The only known signalized intersections in Curry County are along US 101 in Brookings and north of Gold Beach.

BRIDGES AND CULVERTS

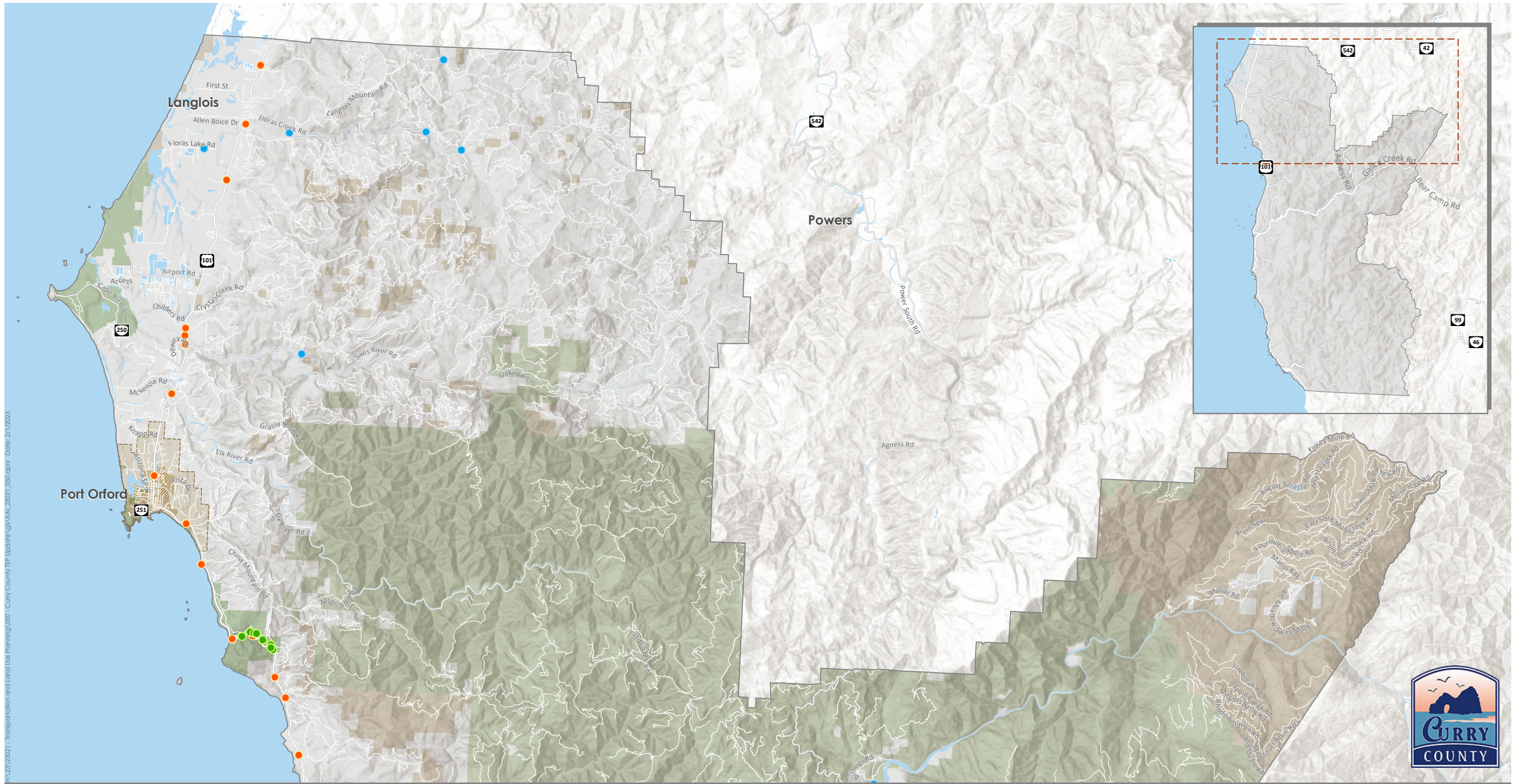
Bridges and culverts are critical elements in the transportation system for continuously conveying multimodal traffic across barriers in the road network, such as rivers, streams, ditches, etc. They are especially vital for freight movement and the overall economy and prosperity of Curry County, Southwest Oregon, and the state. Figure 13 identifies the locations of known existing bridges and culverts within Curry County, according to who owns and maintains them. Given that system resiliency is a priority for the County, it may be critical to identify bridges that could support emergency routes, regardless of County ownership, such as those along USFS roads. The following sections describe the bridges and culverts from Figure 13, including their conditions.

Bridges

Curry County's transportation system includes 75 bridges, and of these, 36 are maintained by the County, including 32 bridges on the National Bridge Inventory System (NBIS). Bridges across the county range from prestressed concrete slab or girder bridges, steel or timber girder bridges, concrete box beam bridges, steel truss bridges, and concrete arch bridges. Some bridges have been identified as structurally deficient, scour critical, weight restricted, load-posted, and/or needing review for Special Haul Vehicle (SHV) loads:

- **Structurally deficient bridges** are those that have a deck, superstructure, or substructure, that are rated as being in poor or worse condition.
- **Scour critical bridges** are those with abutments or pier foundations rated as unstable due to either observed scour at the bridge site or scour potential that is determined from a scour evaluation study.
- **Weight restricted bridges or bridges posted for load** are those that may not have the capacity to safely carry certain loads and/or load combinations, requiring some freight to use alternate routes, if available.
- **SHVs** are closely spaced, multi-axle unit trucks that comply with Federal Bridge Formula B weights and are considered legal but result in higher loads concentrated over shorter distances (e.g., dump trucks, construction vehicles, etc.). *According to ODOT's 2020 Bridge Condition Report, all new load ratings consider SHVs.*

No bridges in the county are currently identified as functionally obsolete or needing review for Emergency Vehicle (EV) loads (but these are also considered in regular load ratings). Table 14 summarizes which bridges in the county are currently structurally deficient, weight restricted, load posted, or needing review for SHV loads. Attachment C tabulates which bridges in the county are scour critical.



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- County Highway Bridge
- State Highway Bridge
- State Park Bridge



Figure 13

**Bridge Jurisdictions
Curry County, Oregon**



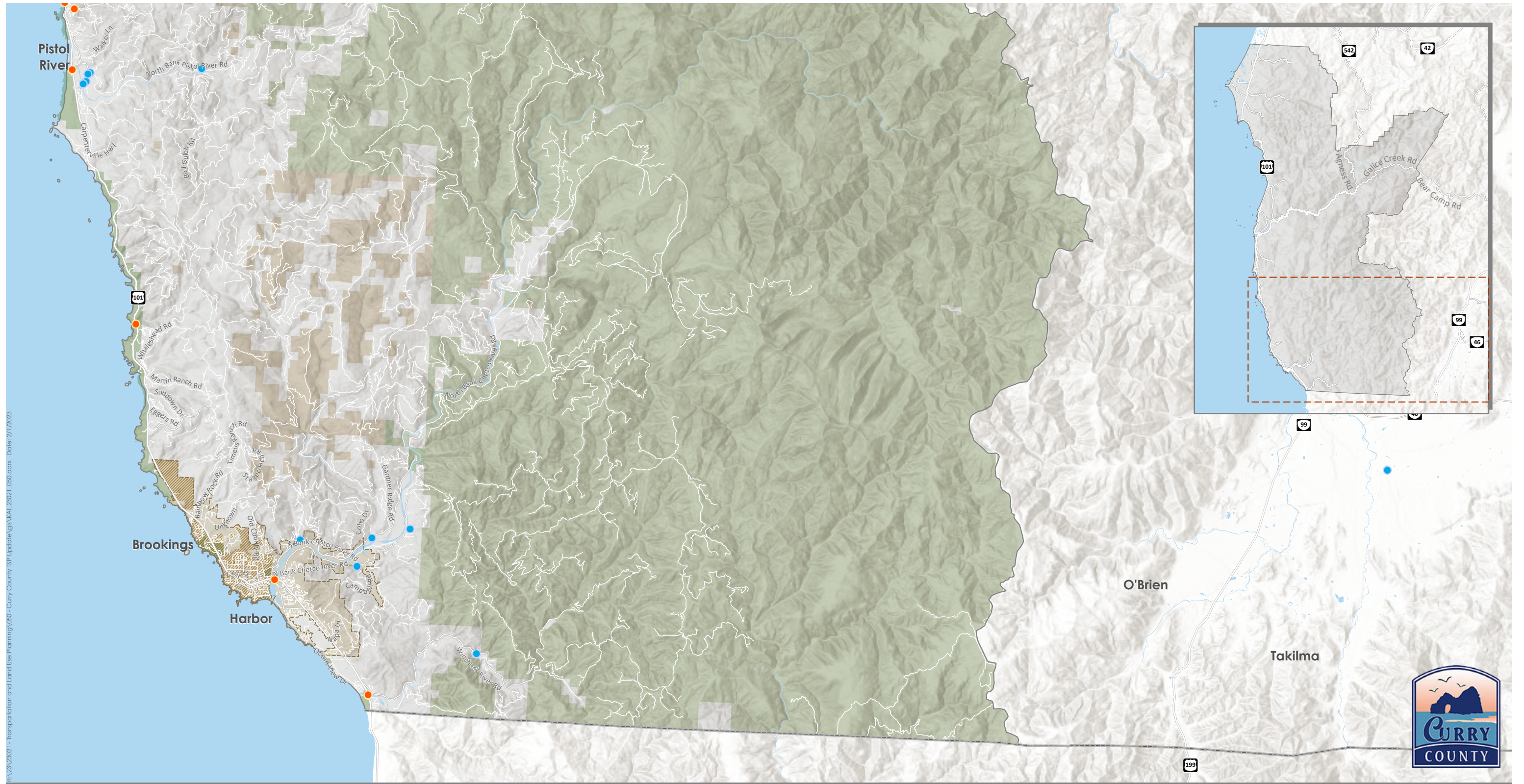
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- County Highway Bridge
- State Highway Bridge
- State Park Bridge



Figure 13

**Bridge Jurisdictions
Curry County, Oregon**



FILE:2/23/2021 - Transportation and Land Use Planning\660 - Curry County TSP Update\gk\VAI_23021_060.aprx Date: 2/17/2023

- County Highway Bridge
- State Highway Bridge
- State Park Bridge



Figure 13

**Bridge Jurisdictions
Curry County, Oregon**

Table 14. Posted/Weight Restricted and SHV Load Review Bridges

Name	ID	Owner	Carries	Crosses	MP	Structurally Deficient	Weight Restricted or Load Posted	SHV Load Review
Winchuck River Rd	20530	County	Winchuck River Rd	Cat Creek	6.00			✓
Jack's Creek	19606	County	S Bank Chetco River Rd	Jack's Creek	3.30			✓
Mill Creek	15C23	County	S Bank Chetco River Rd	Mill Creek	5.70		✓	✓
Pistol River	15C33	County	Pistol River Loop Rd (Co. Rd 693)	Pistol River	0.30	✓		
Deep Creek	15C45	County	North Bank Pistol River Rd	Deep Creek	5.07			✓
Upper Crook Creek	15C32	County	North Bank Pistol River Rd	Upper Crook Creek	0.07			✓
Lower Crook Creek	18060	County	Pistol River Loop Rd (Co. Rd 693)	Lower Crook Creek	0.60			✓
Pistol River	08719	ODOT	US 101	Pistol River	339.10			✓
Myers Creek	00995	ODOT	OR 255	Myers Creek	338.33			✓
Myers Creek	08718	ODOT	US 101	Myers Creek	336.94			✓
Hunters Creek	15C25	County	Hunter Creek Rd	Hunters Creek	3.80			✓
Hunters Creek	15C24	County	Hunter Creek Rd	Hunters Creek	0.90			✓
Hunter Creek	15C010	County	Hunter Creek Rd	Hunter Creek	0.20		✓	
Rogue River	01172	ODOT	US 101	Rogue River	327.70			✓
Indian Creek	15C009	County	Jerry's Flat Rd	Indian Creek	0.70			✓
Saunders Creek	18059	County	Jerry's Flat Rd (Co. Rd 595)	Saunders Creek	3.10			✓
Jim Hunt Creek	15C36	County	Jerry's Flat Rd	Jim Hunt Creek	5.20			✓
Edson Creek	15C006	County	N Bank Rogue River Rd	Edson Creek	0.10			✓
Kimball Creek	15C37	County	Jerry's Flat Rd	Kimball Creek	8.10			✓
Hwy 9 over Conn Rd	16014	ODOT	US 101	Ocean Way (Conn. Rd)	326.47		✓	
Euchre Creek	15C31	County	Ophir Rd (Co. Road 510)	Euchre Creek	0.10		✓	
Mussel Creek	02387A	ODOT	US 101	Mussel Creek	313.15			✓
Myrtle Creek	07720	ODOT	US 101	Myrtle Creek	313.02			✓
Foster Creek	15C29	County	Co. Rd 375	Foster Creek	6.60			✓
Reinhart Creek	07514	ODOT	US 101	Reinhart Creek	311.40		✓	
Myrtle Creek	15C15	County	Arizona Ranch Rd (Co. Road 500)	Myrtle Creek	0.30		✓	
Arizona Beach	20962	State Park	Pedestrian Bridge	Myrtle Creek	0.00	✓		
Brush Creek	02382A	ODOT	US 101	Brush Creek	309.56			✓

Name	ID	Owner	Carries	Crosses	MP	Structurally Deficient	Weight Restricted or Load Posted	SHV Load Review
Bear Trap Creek	02386A	ODOT	US 101	Bear Trap Creek	308.84			✓
Brush Creek Trail (#2)	21516	State Park	Bike/ Pedestrian	Brush Creek	0.00	✓		
Brush Creek	07786	ODOT	Front Rd	Brush Creek	307.02			✓
Brush Creek Trail (#1)	21514	State Park	Bike/ Pedestrian	Creek	0.00	✓		
Humbug Mt Trail Bridge	21518	State Park	Bike/ Pedestrian	Trail	0.00	✓		
Edson Creek	15C004	County	Sixes River Rd	Edson Creek	4.20		✓	
Willow Creek	00910A	ODOT	US 101	Willow Creek	290.38			✓
Floras Creek	15C19	County	Floras Creek Rd	Floras Creek	1.60			✓
North Floras Creek	15C26	County	Floras Creek Rd	Floras Creek (N Fork)	8.90	✓		

Bridge inspectors evaluate these bridges once every two years to rate them based on multiple criteria and assign them with a score referred to as a 'sufficiency rating.' This rating is a numeric evaluation of a bridge's sufficiency to remain in service. Sufficiency ratings range from zero to 100, with zero being entirely insufficient and 100 being entirely sufficient. The sufficiency rating considers structural adequacy, serviceability, functional obsolescence, importance for public use, eligibility for federal replacement funds, and a few lesser factors. Bridges receiving a low score are posted to restrict allowable maximum vehicle weight, rehabilitated, or replaced, depending on the reason for the low score.

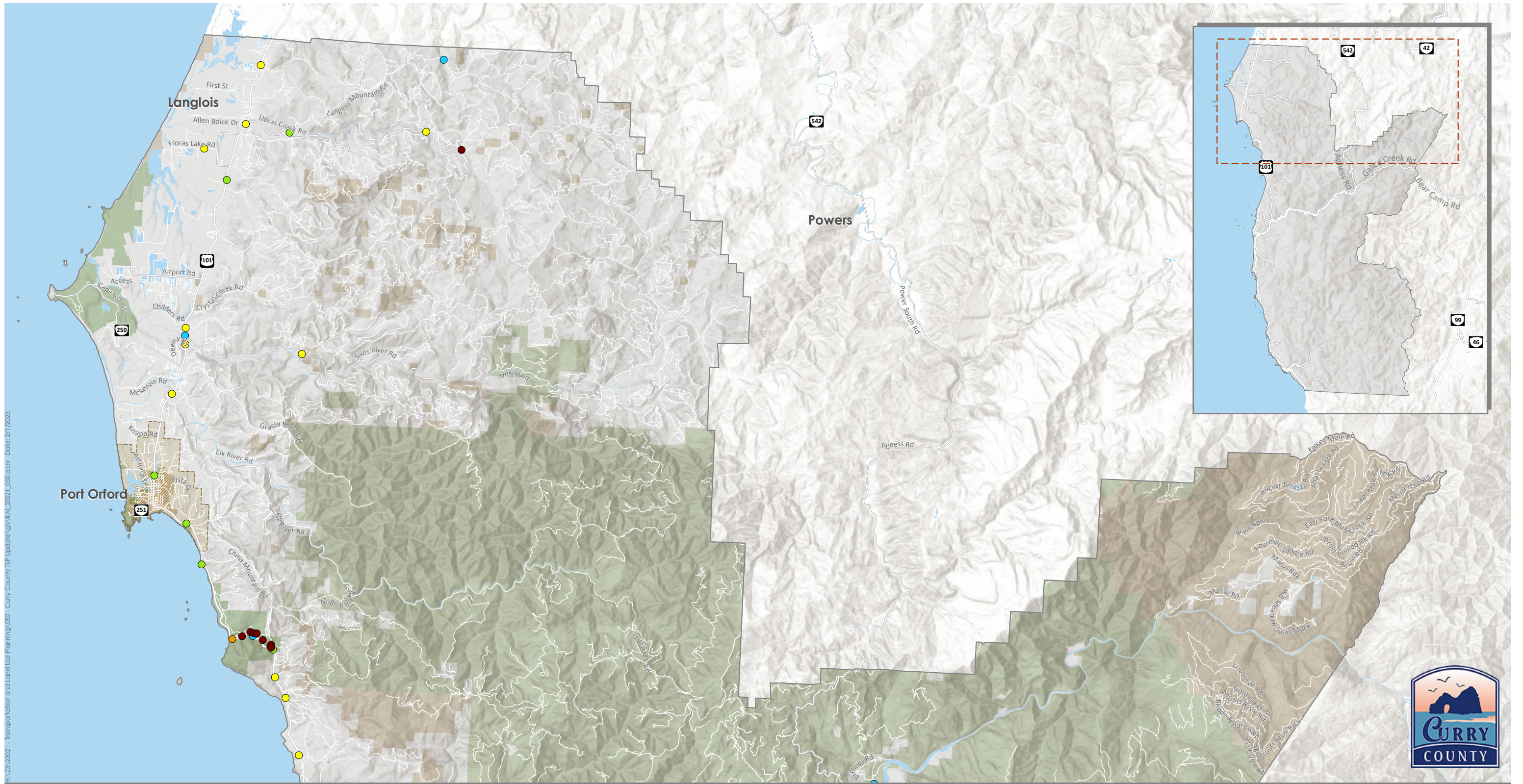
A bridge with a sufficiency rating below 50 indicates that the bridge is in poor condition and is eligible for replacement. Bridges rated between 50 and 80 indicate that the bridge is in fair condition, and that rehabilitation, if cost-effective, will bring the bridge up to current standards. Bridges with sufficiency ratings above 80 may have specific elements that do not meet current minimum standards, but overall are in good or adequate condition and are not eligible for federal funding. The most recent bridge inspection reports provided by ODOT show the following sufficiency ratings:

- 17 bridges have sufficiency ratings above 80;
- 33 bridges have sufficiency ratings between 50 and 80; and,
- 25 bridges have sufficiency ratings below 50.

Figure 14 illustrates the sufficiency rating of all bridges in Curry County. The bridges with sufficiency ratings below 50 are eligible for replacement and are summarized in Table 15.

Culverts

Just over 3,600 culverts support the County's transportation system, and of these, 3,573 are maintained by the County, totaling just under 30 miles worth of infrastructure. Culverts range from corrugated steel or aluminum to precast or prestressed concrete to wood or timber. ODOT identifies which of the culverts supporting its highways are scour critical. This information is summarized in Attachment C along with scour critical bridges.



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Bridge Sufficiency Rating

- 0 - 20
- >20 - 40
- >40 - 60
- >60 - 80
- 80 - 100



Figure 14

**Bridge Sufficiency Ratings
Curry County, Oregon**



Bridge Sufficiency Rating

- 0 - 20
- >20 - 40
- >40 - 60
- >60 - 80
- 80 - 100



Figure 14

**Bridge Sufficiency Ratings
Curry County, Oregon**



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Bridge Sufficiency Rating

- 0 - 20
- >20 - 40
- >40 - 60
- >60 - 80
- 80 - 100



Figure 14

**Bridge Sufficiency Ratings
Curry County, Oregon**

Table 15. Bridges in Curry County with Sufficiency Ratings Below 50

Name	ID	Owner	Carries	Crosses	MP	Sufficiency Rating
Arizona Beach	20962	State Park	Pedestrian Bridge	Myrtle Creek	0.00	Unknown
Brush Creek Trail (#2)	21516	State Park	Bike/ Pedestrian	Brush Creek	0.00	Unknown
Brush Creek Trail (#1)	21514	State Park	Bike/ Pedestrian	Creek	0.00	Unknown
Humbug Mt Trail Bridge	21518	State Park	Bike/ Pedestrian	Trail	0.00	Unknown
US 101 Sign Cantilever (SB)	22369	Private (Non-Railroad)	Sign Cantilever	US 101	354.97	Unknown
US 101 Sign Cantilever (NB)	22373	Private (Non-Railroad)	Sign Cantilever	US 101	355.27	Unknown
Humbug Day Use Area	19783	State Park	Park Rd	Brush Creek	307.02	Unknown
Humbug Mt. Pedestrian Bridge	21005	State Park	Park Rd	Brush Creek	0.00	Unknown
Brush Creek Trail Bridge No. 3 (South End)	21515	State Park	Bike/Pedestrian	Brush Creek	0.00	Unknown
Ped Brush Creek Humbug Mtn Park Trail	22713	State Park	Humbug Mtn Trail	Brush Creek	0.00	Unknown
North Floras Creek	15C26	County	Floras Creek Rd	Floras Creek (N Fork)	8.90	17.1
Euchre Creek	15C31	County	Ophir Rd (Co. Road 510)	Euchre Creek	0.10	22.9
Brush Creek	18096	ODOT	US 101	Brush Creek	306.35	29.8
Pistol River	15C33	County	Pistol River Loop Rd (Co. Rd 693)	Pistol River	0.30	30.1
Myrtle Creek	15C15	County	Arizona Ranch Rd (Co. Road 500)	Myrtle Creek	0.30	34.3
Rogue River	01172	ODOT	US 101	Rogue River	327.70	37.4
Hunter Creek	15C010	County	Hunter Creek Rd	Hunter Creek	0.20	37.7
Winchuck River	09091A	ODOT	US 101	Winchuck River	362.61	42.0
N Fork Chetco River	15C14	County	N Bank Chetco River Rd	N Fork Chetco River	5.30	43.1
Hunters Creek	15C24	County	Hunter Creek Rd	Hunters Creek	0.90	44.3
Willow Creek	15C12	County	Co. Road 136	Willow Creek (EB)	0.40	44.8
Floras Creek	09370	ODOT	US 101	Floras Creek	288.50	46.3
Elk River	00902C	ODOT	US 101	Elk River	297.37	46.4
Bear Trap Creek	02386A	ODOT	US 101	Bear Trap Creek	308.84	47.5
Morton Creek	00912	ODOT	US 101	Morton Creek	286.61	48.9

Bridge and Culvert Findings

- Curry County's transportation system includes 75 bridges and over 3,600 culverts. The County maintains 36 bridges – of which, 32 are on the NBIS – and 3,573 culverts.
- No bridges in the county are currently identified as functionally obsolete or needing review for EV loads, but 6 bridges have been identified as structurally deficient, 7 bridges are weight restricted or load posted, 25 bridges have sufficiency ratings below 50, 25 bridges are identified for needing review for SHV loads, and 47 bridges and 23 culverts are scour critical. In some instances, many of these characteristics apply to one bridge.

BICYCLE AND PEDESTRIAN NETWORK

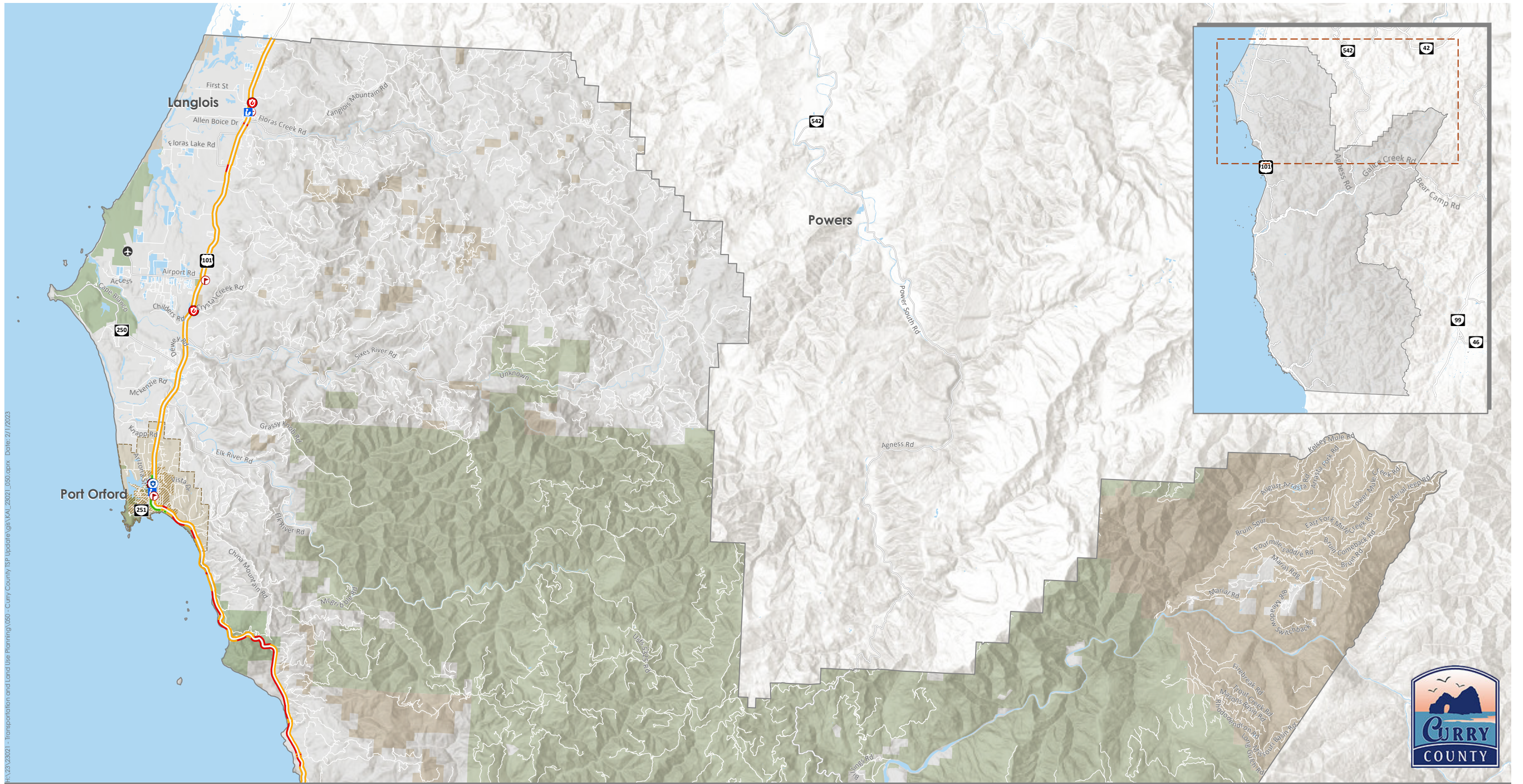
Bicycle and pedestrian facilities are the elements of the transportation system that enable people to bike and walk safely and efficiently between land uses. Within Curry County, bicycle and pedestrian facilities primarily serve short trips between major attractors within the urban areas, but cyclists can travel along nearly all of US 101 via paved shoulders. In general, biking and walking can be viable travel options for Curry County residents when supported by facilities such as shoulder bikeways, bike lanes, sidewalks, shared-use paths, and trails, as well as secure bicycle parking, work-place showers, and bus-mounted bike racks, or when mixed-use developments give people the option to live near their place of employment.

Bicycle Facilities

Figure 15 presents a map of bicycle facilities that are available on State and County roadways. As evinced, bicycle facilities are limited to only short stretches of striped bike lanes within the urban areas, largely on US 101; these sections include:

- US 101 in Port Orford between Port Orford Loop and Jackson Street
- US 101 in Gold Beach south of the Isaac Lee Patterson Bridge
- Jerry's Flat Road north of Saunders Creek Road
- US 101 in Brookings between Easy Street and Elk Drive and between Oak Street and the Chetco River Bridge
- W Benham Lane between Ocean View Drive and US 101
- Lower Harbord Road between US 101 and Ocean View Drive
- Boat Basin Road for its entire extent
- S Bank Chetco River Road between US 101 and Harbor View Circle
- Oceanview Drive between east of Cedar Lane and US 101

The rest of the bicycle network is comprised of paved shoulders along US 101 and few County facilities that are at least four feet wide. For US 101, these shoulders are present along most of the roadway, with some gaps on one or both sides of the road throughout. In these stretches, bicyclists must share the travel lanes with vehicles, where posted speeds are generally 55 MPH. Similarly, other than for the short segment of four-foot shoulders along Jerry's Flat Road, bicyclists must share the travel lanes with vehicles on the remaining State highways and almost all of the County's arterial and collector network.



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- Bike Lanes
- 4 Foot or Greater Paved Shoulders
- Less than 4 Foot Paved Shoulders
- + Parks

- Essential Destinations**
- | | |
|--------------|-----------------|
| Airport | Law Enforcement |
| City Hall | Library |
| Fire Station | Medical |
| Hospital | School |



Figure 15
Bicycle Facilities
 Curry County, Oregon



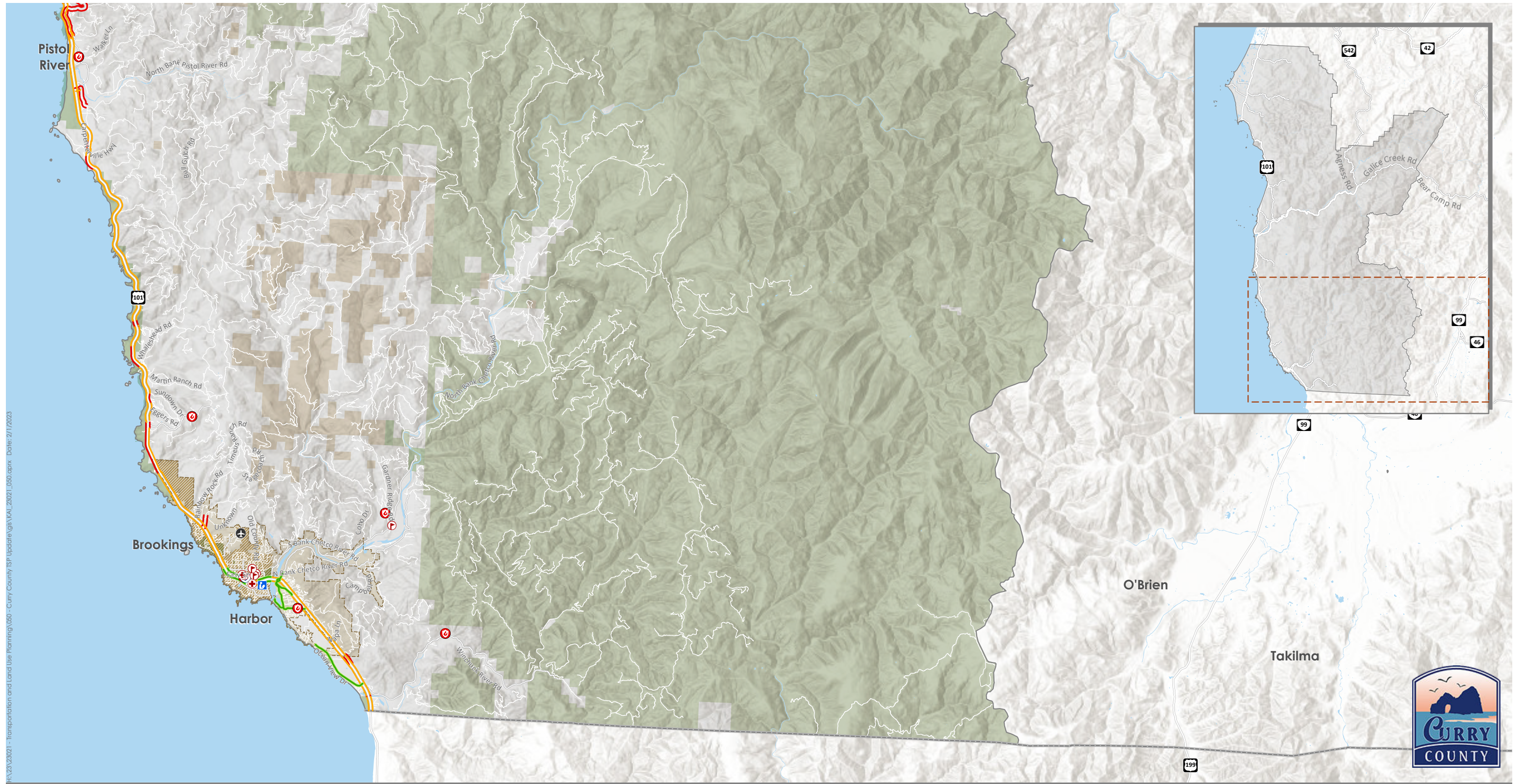
FILED: 2/23/2021 - Transportation and Land Use Planning\660 - Curry County TSP Update\fig15_VA_23021_060.aprx Date: 2/1/2023

- Bike Lanes
- 4 Foot or Greater Paved Shoulders
- Less than 4 Foot Paved Shoulders
- + Parks

- Essential Destinations**
- | | |
|--------------|-----------------|
| Airport | Law Enforcement |
| City Hall | Library |
| Fire Station | Medical |
| Hospital | School |



Figure 15
Bicycle Facilities
Curry County, Oregon



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- Bike Lanes
- 4 Foot or Greater Paved Shoulders
- Less than 4 Foot Paved Shoulders
- + Parks

Essential Destinations

- | | |
|--------------|-----------------|
| Airport | Law Enforcement |
| City Hall | Library |
| Fire Station | Medical |
| Hospital | School |



Figure 15

**Bicycle Facilities
Curry County, Oregon**

Note that US 101 is a coastal bike route. The Oregon Coast Bike Route (OCBR) is a popular bike route running the length of the Oregon coast for approximately 370 miles. It is estimated that between 6,000 and 10,000 people ride the OCBR annually.

Further, the Wild Rivers Coast Scenic Bikeway, an Oregon Scenic Bikeway, is a 60-mile route starting and ending in Port Orford with scenic stops in State Parks and other destinations. The route extends between the Cape Blanco Lighthouse, in and around Port Orford, and due east along Elk River Road.

Pedestrian Facilities

Figure 16 presents a map of pedestrian facilities that are available on State and County roadways. As evinced, pedestrian facilities are limited to only short stretches of sidewalks on one or both sides of the road, largely on US 101 and other facilities in urban areas; these sections include:

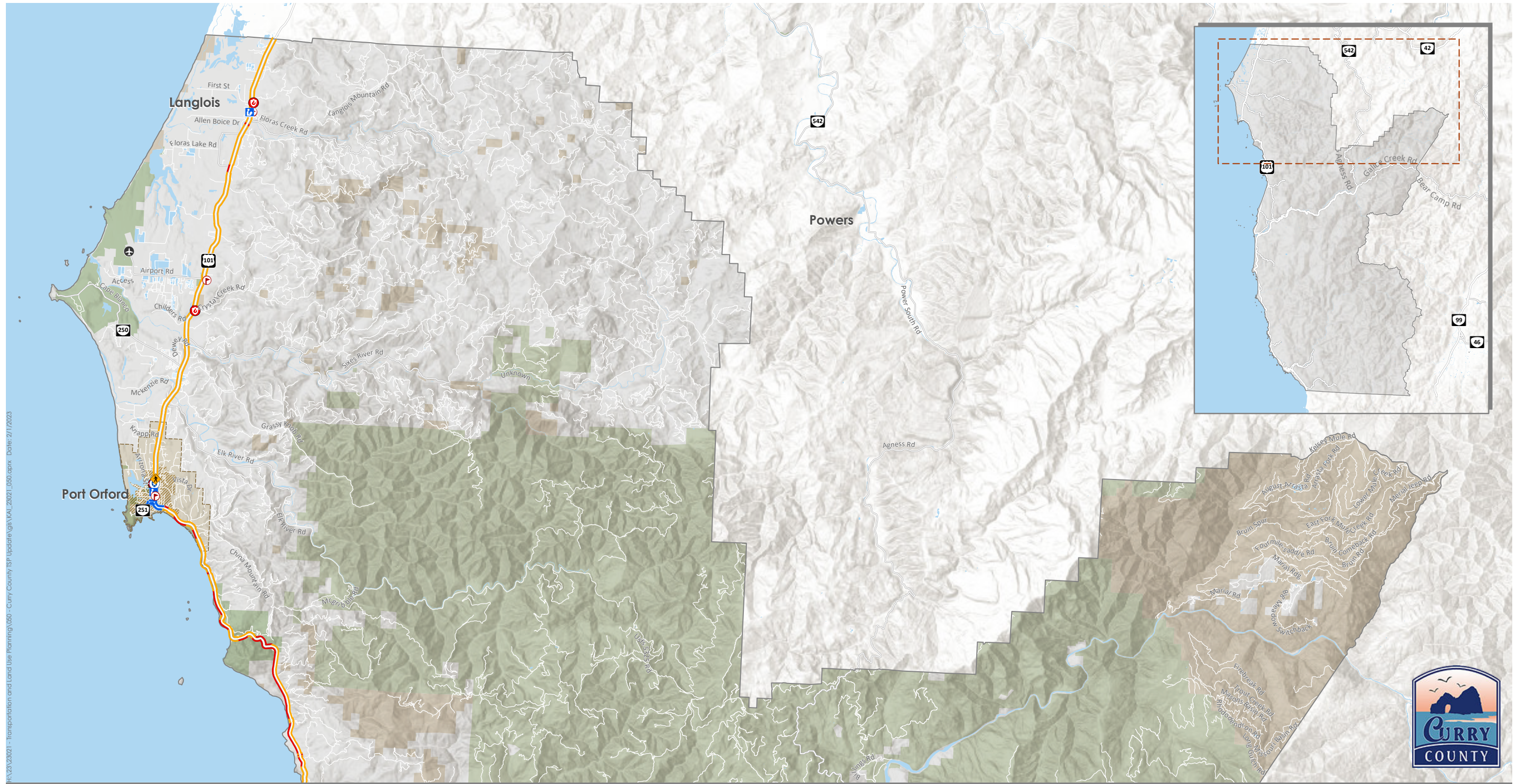
- US 101 in Port Orford between Port Orford Loop and Jackson Street
- US 251 in Port Orford
- US 101 in Gold Beach south of the Isaac Lee Patterson Bridge
- OR 255 between US 101 and De Moss Road
- US 101 between Parkview Drive and W Benham Lane
- Shopping Center Avenue between Lower Harbor Road and W Hoffeldt Lane
- W Hoffeldt Lane between Lively Lane and US 101
- Lower Harbor Road between Shopping Center Avenue and US 101
- S Bank Chetco River Road between US 101 and Harbor View Circle
- W Benham Lane between Lower Harbor Road and US 101

Outside of these sidewalks, pedestrians are limited to using the minimum four-foot paved shoulders along US 101, Jerry's Flat Road, Lower Harbor Road, and Oceanview Drive. Otherwise, pedestrians must use the roadway edge or gravel shoulder, if available. As with bicycles, high speeds on the arterial and collector network make these walking environments uncomfortable and unsafe for pedestrians.

Marked crosswalks are provided only in Brookings, Gold Beach, and Port Orford, mostly across US 101. According to ODOT, the majority of these crosswalks connect to curb ramps that are Americans with Disabilities Act (ADA) compliant.

Activity Centers

Many of Curry County's primary activity centers that could generate biking and walking trips, shown in Figure 15 and Figure 16, are located within the urban and unincorporated areas and appear to be accessible by bicycle or walking. Outside of the urban areas, bicyclists are generally able to access recreational sites and similar destinations along the coast using the paved shoulders along US 101 (a coastal bike route). The same is true for pedestrians, although this is not an ideal walking environment. If bicyclists and pedestrians want to access similar destinations throughout Curry County east of US 101, they must share roadway travel lanes with motorists or use the roadway edge or gravel shoulders, if available. While there likely are pathways in and around recreational areas in the county, no data are currently available.



FILE: 2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/fig16_VA_23021_060.oprx Date: 2/17/2023

- Sidewalks
- 4 Foot or Greater Paved Shoulders
- Less than 4 Foot Paved Shoulders
- Parks

- Essential Destinations**
- | | |
|--------------|-----------------|
| Airport | Law Enforcement |
| City Hall | Library |
| Fire Station | Medical |
| Hospital | School |



Figure 16

**Pedestrian Facilities
Curry County, Oregon**



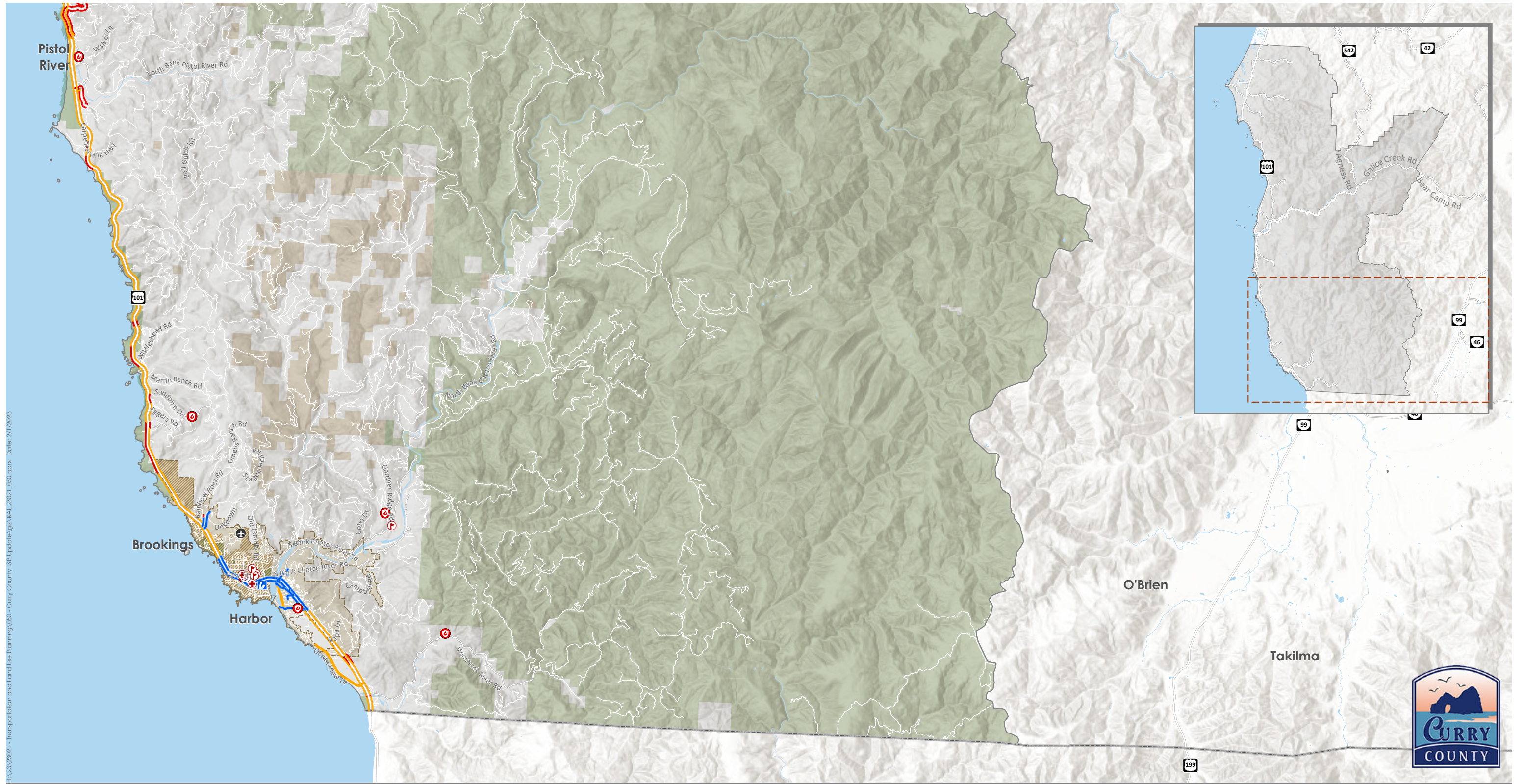
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- Sidewalks
- 4 Foot or Greater Paved Shoulders
- Less than 4 Foot Paved Shoulders
- + Parks

- Essential Destinations**
- | | |
|--------------|-----------------|
| Airport | Law Enforcement |
| City Hall | Library |
| Fire Station | Medical |
| Hospital | School |



Figure 16
**Pedestrian Facilities
 Curry County, Oregon**



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- Sidewalks
- 4 Foot or Greater Paved Shoulders
- Less than 4 Foot Paved Shoulders
- + Parks

Essential Destinations

- | | |
|---|---|
| ✈ Airport | 👮 Law Enforcement |
| 🏛 City Hall | 📖 Library |
| 🚒 Fire Station | 🏥 Medical |
| 🏥 Hospital | 🎓 School |



Figure 16

**Pedestrian Facilities
Curry County, Oregon**

Bicycle and Pedestrian Network Findings

- County collectors and arterials lack walking and biking facilities, including paved shoulders or pathways in the rural areas and sidewalks or bike lanes within city UGBs, except for some sidewalks and bike lanes on select streets in Brookings and Gold Beach.
- The only walking and biking facilities available on the State highway system are along US 101 and are primarily comprised of paved shoulders that are four feet or greater in width. Sidewalks and bike lanes are generally provided on US 101 within the UGBs.
- The Oregon Coast Bike Route (OCBR) is a popular bike route running the length of the Oregon coast for approximately 370 miles. The Wild Rivers Coast Scenic Bikeway surrounds the Port Orford area with scenic stops in State Parks and other destinations.
- Many of Curry County's primary activity centers that could generate biking and walking trips are located within the urban and unincorporated areas and appear to be accessible by bicycle or walking.

PUBLIC TRANSIT

Curry County is served by two public transit providers – Curry Public Transit (CPT), who is the primary transit service provider, and the SouthWest POINT. Figure 17 illustrates the service routes of these providers in the county. In addition, several specialized transportation providers offer medical transportation within the county. CPT travels outside of the county so riders can connect to Redwood Coast Transit, and Coos County Area Transit District to reach destinations in Coos Bay, Medford, Northern California, and beyond.

The following sections provide more detail on the fixed-route and specialized transportation services within the county and the transit facilities that are available. They also describe the populations throughout Curry County that may be considered transportation disadvantaged, relying on transportation modes other than a personal vehicle, such as public transit.

Existing Services

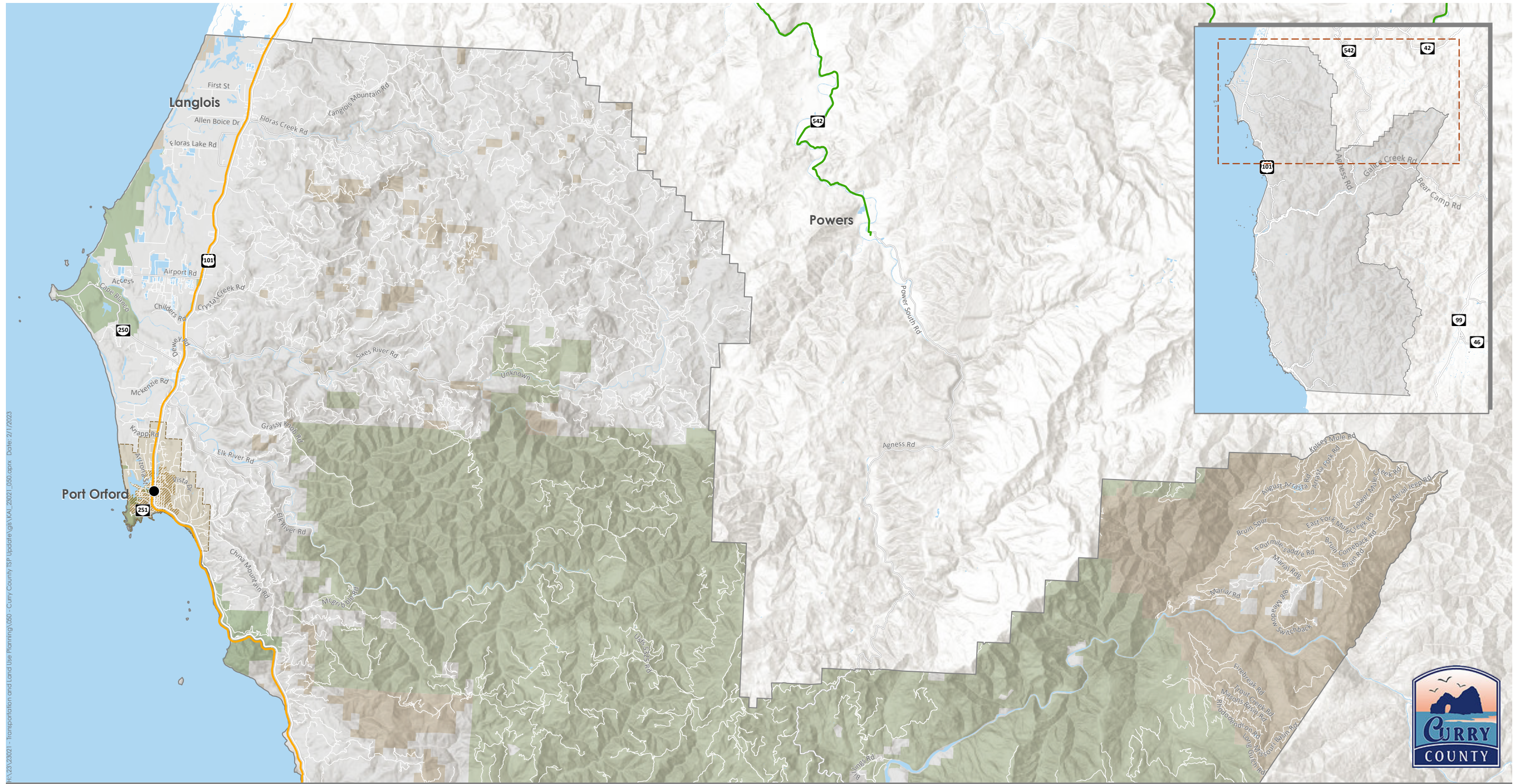
The following sections describe the routes and services offered by CPT and the Southwest POINT as well as the specialized transportation services available within the county.

Curry Public Transit

Curry Public Transit (CPT) operates the Coastal Express fixed-route service from Coos Bay/North Bend in Coos County to Smith River in northern California, as well dial-a-ride in Brookings and Gold Beach. Key information about these services is described below.

CPT Coastal Express

The Coastal Express, CPT's fixed-route service, operates from 6:15 AM to 6:55 PM, Monday through Saturday. Service is not provided on Sunday and holidays. Fares are \$4.00 per city segment, in which a city segment is defined as beginning prior to reaching the city limit of where a designated stop exists and ending at the city limit of that city. Free service is provided for active-duty military personnel, veterans with an appointment card for the Brookings or Coos Bay Veterans Affairs (VA) clinic, or children aged 6 and under. A reduced fare of \$2.00 per city segment is available to passengers aged 60 or older, currently-enrolled students, and persons with a documented disability.



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- Transit Stops
- Coos County Area Transit
- Curry Public Transit



Figure 17

**Public Transportation Routes and Stops
Curry County, Oregon**



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- Transit Stops
- Curry Public Transit
- SouthWest POINT



Figure 17

**Public Transportation Routes and Stops
Curry County, Oregon**



FILE:2/23/2021 - Transportation and Land Use Planning/660 - Curry County TSP Update/fig17_VA1_23021_060.aprx Date: 2/17/2023

- Transit Stops
- Curry Public Transit
- SouthWest POINT



Figure 17

**Public Transportation Routes and Stops
Curry County, Oregon**

CPT Dial-A-Ride

Dial-a-Ride is origin-to-destination, wheelchair accessible, demand-response service operating within the cities of Brookings and Gold Beach. In Brookings, Dial-a-Ride operates from 8:00 AM to 5:00 PM Monday through Friday and 9:00 AM to 4:00 PM on Saturday (temporarily suspended), with no Sunday service.

In Gold Beach, Dial-a-Ride operates from 9:00 AM to 2:00 PM on Monday and Tuesday and from 9:00 AM to 4:00 PM Wednesday and Friday, with no Thursday or weekend service. Buses run south on the hour and north on the half hour, with a half hour pick up window after scheduled time. Fares are \$4.00 per ride; \$2.00 per ride for seniors and people with disabilities; and free for veterans and kids.

SouthWest Point

The Point, Oregon's intercity bus service contracted by Pacific Crest Lines, offers the SouthWest route between Klamath Falls and Brookings, with connections in Ashland, Medford, Grants Pass, Cave Junction, O'Brien, Gasquet, Hiouchi, Crescent City, and Smith River. There are two segments: the Klamath Falls to Grants Pass segment operates seven days a week and on holidays; the Cave Junction to Brookings segment operates Monday through Saturday and on holidays. The service offers one departure in each direction per day. The first departs Klamath Falls at 10:00 AM and arrives in Brookings at 5:25 PM, the second departs Brookings at 10:45 AM and arrives in Klamath Falls at 7:30 PM. Two fare tickets are needed to travel through both segments of the entire SouthWest Route. Free local connecting service between Grants Pass and Cave Junction is currently provided by Josephine Community Transit (JCT), with a stop in Selma.

Specialized Transportation Services

Curry County has several transportation providers that serve special populations. Typically, these services are limited to medical transportation for individuals with specific transportation challenges, such as the elderly or persons with disabilities. These medical transportation providers are described below.

ReadyRide

AllCare – one of two Coordinated Care Organizations (CCOs) in Curry County – provides non-emergency medical transportation (NEMT) rides to AllCare members through ReadyRide. Transportation is provided free of charge to eligible clients living in Curry, Douglas, Jackson, and Josephine counties who have no other way to get to their medical services. Rides can be provided 24 hours a day, seven days a week, but must be scheduled at least two days in advance during office hours, Monday through Friday 8:00 AM to 6:00 PM.

TransLink

Non-emergency medical transportation for Oregon Health Plan (Medicaid) recipients is an important specialized service need. The Rogue Valley Transportation District (RVTD) manages the TransLink brokerage service for Western Oregon Advanced Health CCO members living in Jackson, Josephine, Douglas, Coos, Curry, Lake, and Klamath counties, as well as members of other CCOs living in Jackson, Josephine, and Klamath counties. Transportation is provided free of charge to eligible clients living in Coos and Curry counties who have no other way to get to their medical services. Rides can be provided 24 hours a day, seven days a week, but must be scheduled at least two days in advance during office hours, Monday through Friday 7:00 AM to 5:00 PM.

Existing Facilities

The following sections describe the various facilities that are available to transit riders in Curry County, including bus stops and their amenities and connections to walking and biking facilities, park-and-rides, and transit centers.

Bus Stops and Amenities

The CPT fixed route includes five bus stops in the county: one in Port Orford, one in Gold Beach, one in Brookings, and two in Harbor. In addition, CPT allows flag stops along US 101 if the driver can pull off the road safely. Figure 17 illustrates the locations of these bus stops along the fixed route within Curry County. Bus stop amenities increase the comfort of riders while they wait to board buses. Amenities can include stop signage, bus shelters, benches, timetables, trash receptacles, bike racks, and more.

The five bus stops in Curry County include the following amenities, if any:

- Langlois Public Library (Flag Stop)
 - No amenities
- Langlois Store (Flag Stop)
 - No amenities
- Ray's Food Place, Port Orford
 - Covered shelter and waiting area
 - Bench
 - Trash Receptacle
- Ray's Food Place, Gold Beach
 - CPT bus stop sign
 - Covered shelter and waiting area
 - Bench
 - Street lighting
- 5th Street / Bankus Park, Brookings
 - CPT bus stop sign
 - Covered shelter and waiting area
 - Bench
 - Bike racks
- Chevron Station, Harbor
 - Temporary CPT bus stop sign
- McKay's Market, Harbor
 - No amenities

Park and Rides

Park-and-ride lots are typically feasible in situations where there is either a parking charge or parking shortages at the rider's destination, or if there is a substantial savings in travel cost or time by using transit. CPT does not currently have park and ride facilities as parking in many areas is free and widely available.

Transit Centers

No transit centers are present in the CPT service area. Currently, the main "station" is in Brookings. Depending on how transit service evolves in Brookings over the long term, a larger, on- or off-street site may become necessary to facilitate connections between intercity, local circulator, and dial-a-ride services. The closest potential transit center could be located at the VA Clinic/Safeway stop in North Bend, which is planned by Coos County Area Transit (CCAT) to be a "mobility hub" site, where multiple modes connect.

Transportation Disadvantaged Populations

CPT recently adopted its Transit Development Plan (TDP), which includes a detailed evaluation of transportation disadvantaged populations throughout Curry County according to 2019 American Community Survey (ACS) data. The following summarizes the findings from the TDP on disabled populations in the county.

- 24% of Curry County's overall population lives with a disability (as compared to 14% of the State's population).
- 17% of the population in Brookings lives with a disability.
- 23% of the population in Gold Beach lives with a disability.
- 40% of the population in Port Orford lives with a disability.
- 43% of the population in Harbor lives with a disability.
- 24% of the population in Langlois lives with a disability.
- 37% of the population in Nesika Beach lives with a disability.
- 36% of the population in Pistol River lives with a disability.

As shown, Curry County has a significantly higher percentage of persons living with a disability than Oregon overall, with the highest percentages located in the Harbor area and lowest in the incorporated city of Brookings.

Public Transit Findings

- Curry County is served by two public transit providers – CPT and the SouthWest POINT. CPT is the primary provider in the county and operates fixed-route service between Coos Bay/North Bend and Smith River, as well as dial-a-ride service in Brookings and Gold Beach. SouthWest POINT operates intercity bus service between Klamath Falls and Brookings. Specialized transportation services are also available through ReadyRide and TransLink.
- The CPT fixed route includes five official bus stops in the county (1 in Port Orford, 1 in Gold Beach, 1 in Brookings, and 2 in Harbor) and 2 flag stops in Langlois. The amenities at these bus stops vary from no amenities to covered shelters, signs, benches, and bike racks.

- CPT does not currently have park and ride facilities or transit centers in the CPT service area.
- 24% of Curry County's overall population lives with a disability (as compared to 14% of the State's population). Many of these populations are concentrated around the incorporated cities and unincorporated rural communities.
- The recently adopted CPT TDP update has identified several transit needs, including service improvements to key destinations with a focus on Title VI populations; increased level of service for the Coastal Express; maintained and expanded/improved Dial-A-Ride service; expanded service to targeted areas; opportunities with healthcare organizations; regional connectivity (Crescent City, Grants Pass, Coos Bay); current/future development needs in Brookings/Harbor, Gold Beach, and Port Orford; and marketing, bus stop amenities, technology, and workforce hiring.

RAIL

No rail transportation is present within Curry County. The closest railroads are Central Oregon & Pacific Railroad (CORP), to the east, and Coos Bay Rail Line (CBRL), to the north.

- CORP: this is a 362-mile Class II ("regional") railroad between Black Butte, California and Springfield, Oregon that generally runs parallel with the Interstate 5 (I-5) corridor. The railroad serves approximately 17,000 cars per year carrying primarily logs, lumber products, and plywood. It has interchanges with Flat Iron Rail in Montague, California, Rogue Valley Terminal Railroad in White City, Oregon, and Union Pacific in Eugene, Oregon and Black Butte, California.
- CBRL: this 134-mile Class III ("short-line") railroad is owned and operated by the Port of Coos Bay and runs between Coquille in Coos County to Eugene in Lane County and averages 5,000 rail carloads per year. Approximately 89 percent of the product that the rail line moves is related to the timber industry, and products move all around the US and to Canada and Mexico, after reaching the Union Pacific interchange near Eugene and connecting to the National Railway network.

Rail Findings

- No rail transportation is present within Curry County. The closest railroads are CORP to the east and CBRL to the north.

AIR, WATER, AND PIPELINE

The following sections describe the air, water, and pipeline transportation facilities within Curry County.

Air Transportation

Curry County is served by three general aviation public airports, including the Brookings Airport, the Gold Beach Municipal Airport, and the Cape Blanco State Airport. The locations of these airports within the county are illustrated in the previous Figure 15 and Figure 16. None of these

airports have helicopter pads. The Oregon Aviation Plan (OAP – Reference 6), prepared by the Aeronautics Division of ODOT, addresses these three airports.

The OAP assigns all public airports to the following five categories:

- Category I: Commercial Service Airports – Commercial service airports typically service a larger geographic area when compared to general aviation airport because people are willing to travel greater distances to access the national air system. The Southwest Oregon Regional Airport in North Bend (Coos County) is the closest Category I airport to Curry County.
- Category II: Urban General Aviation Airports – Urban general aviation airports accommodate all general aviation aircraft and incorporate aviation activity, including business jets, helicopters, and other general aviation activity. The Newport Municipal Airport (Lincoln County) is the closest Category II airport to Curry County.
- Category III – Regional General Aviation Airports – Regional general aviation airports support most twin- and single-engine aircraft and may accommodate occasional business jet use. These airports support a regional transportation need. The Bandon State Airport is the closest Category III airport to Curry County.
- Category IV: Local General Aviation Airports – Local general aviation airports support primarily single-engine, general aviation aircraft, but are capable of accommodating smaller twin-engine general aviation aircraft. These airports support local air transportation needs and special use aviation activities. The Brookings Airport and Gold Beach Municipal Airport are Category IV airports.
- Category V: Remote Access/Emergency Service (RAES) Airports – RAES airports support primarily single-engine, general aviation aircraft, special use aviation activities, and access to remote areas or provide emergency services access. The Cape Blanco State Airport is a Category V airport.

Brookings Airport

Curry County owns this airport, which is located about one mile northeast of Brookings. It has one paved runway that is 2,900 feet long by 60 feet wide. The Brookings Airport includes Fixed Base Operator (FBO) and fuel services and has an air ambulance based on site.

The 2019 OAP recommends that the airport be reclassified from a Category IV to a Category III airport. The airport is a Tier 3 airport in the OAP, out of three tiers, because of its capabilities and for making future investments, based on the forecast needs of their surrounding population and their provision of economic and commercial restoration to the entire region after a disaster event. The OAP identifies improvements for the Brookings Airport, including those related to taxiway lighting, runway length, and runway pavement strength. The OAP's special considerations and recommendations sections focus on the role of this and other Curry County airports in resiliency and natural disaster response in the county. The Brookings Airport is situated outside "coastal hazard areas" (liquefaction and tsunami zones) and is included in the State's 2013 Resilience Plan.

The Brookings Airport is a Tier 2 airport in the 2013 Oregon Resilience Plan (ORP), which is a tier that represents a larger network of airports – as compared to a more limited number of Tier 1 airports – that provide access to most rural areas and will be needed to restore major commercial operations following a natural disaster. It has a "severe" Cascadia Event Hazard

ranking (one below the highest ranking of “violent”) in the ORP but is situated outside of known tsunami hazard areas.

Gold Beach Municipal Airport

This Category IV airport is owned by the Port of Gold Beach and is located west of Gold Beach along the coast. It has one paved runway that is 3,237 feet long by 75 feet wide. The Gold Beach Municipal Airport includes FBO and fuel services. This airport does not fall within the three tiers of the OAP or the Oregon Resilience Plan, has a “violent” Cascadia Event ranking (the highest ranking), and is inside known tsunami hazard areas.

Cape Blanco State Airport

The State owns this airport, which is located four miles northwest of Sixes. Its primary paved runway is 5,100 feet long by 150 feet wide. The Cape Blanco State Airport includes FBO and fuel services and has an air ambulance based on site.

The 2019 OAP recommends that the Cape Blanco State Airport also be reclassified from a Category V to a Category IV airport. The airport is a Tier 1 airport in the OAP based on how it can support recovery efforts related to a disaster event. The Cape Blanco State Airport is included in the 2013 ORP and is situated outside “coastal hazard areas” (liquefaction and tsunami zones). In this vein, the OAP specifically recommends that the airport be included in the National Plan of Integrated Airport Systems (NPIAS) in order to improve geographic coverage of Oregon by NPIAS airports, particularly for resiliency. However, the OAP does not otherwise identify specific improvements for the Cape Blanco State Airport.

The Cape Blanco State Airport is a Tier 2 airport in the ORP. It has a “violent” Cascadia Event Hazard ranking but is located outside of known tsunami hazard areas, as affirmed in the 2019 OAP.

Del Norte Airport

While not in Curry County, the 2019 OAP identifies Del Norte Airport in Crescent City, California, as an out-of-state airport that those in populated parts of Curry County can reach within 120 minutes. (Note: The OAP assesses system performance using a number of criteria, including airports accessible within 30 minutes and 120 minutes.)

County staff on the Project Management Team (PMT) noted that Del Norte Airport is an integral component of Curry County's air transport system. The Board of Commissioners is a member of the Airport Committee. The airport is not acknowledged in the air service section of the 2005 TSP.

Southwest Oregon Regional Airport

Also not in Curry County, but important to the Curry County community, the Southwest Oregon Regional Airport in North Bend (Coos County) is open for public use providing facilities for commercial, general aviation, cargo, and military air traffic. The OAP designates it as a Category I airport.

Service Areas

There are no commercial flights offered out of the Brookings Airport, Gold Beach Municipal Airport, Cape Blanco State Airport, or Del Norte Airport. The Medford-Rogue Valley International Airport is the closest international airport and the third largest commercial service airport in Oregon. From Medford, direct connections are available to Portland, Seattle, San Francisco, Salt Lake City, and other destinations.

Current and Expected Levels of Use

The Brookings Airport currently accommodates 22,600 operations on an annual basis, the Gold Beach Municipal Airport accommodates 5,500 operations, and the Cape Blanco State Airport accommodates 900 operations.

Overlays

Curry County employs an Airport Overlay Zone that governs land uses at the Brookings Airport and Gold Beach Municipal Airport as well as their immediate vicinities. The overlay zone prescribes the clear zone where permitted uses are limited. Height limitations in the overlay zone ensure that such land uses do not interfere with approach, transitional, horizontal, or conical surfaces of the airports. No land uses, exterior materials, or lights are allowed that would interfere with communication or visibility between aircraft and the airports.

Water Facilities

According to the Oregon Department of State Lands' Waterways and Wetlands, the Rogue River and Chetco River are the only navigable waterways within Curry County. The Rogue River runs from east to west from its headwaters near Crater Lake to the river's mouth on the Oregon Coast in Gold Beach. The Chetco River runs from east to west from its headwaters near Chetco Peak in Curry County to the river's mouth on the Oregon Coast in Brookings and Harbor. Other significant waterways within Curry County include the Winchuck River, Hunter Creek, and Pistol River. These rivers are not used for transportation, but for recreational purposes.

In addition, three ports are located in the county in Port Orford, Gold Beach, and Brookings:

- Port of Port Orford: this port is an open-water dock (no natural protection) that supports commercial and visitor fishing with the only dry-dock port, or "dolly dock" on the west coast. There is no ocean bar to cross in this port, which results in more fishable days per year. In addition to fishing, recreational activities such as diving and kayaking are also popular at this port.
- Port of Gold Beach: this port is located near the mouth of the Rogue River and is for tourists, residents, and commercial fisherman. Port services include RV, boat, and trailer storage, marine fuel, and forklift operations and a launch ramp, recreation docks, and commercial docks are all available. Jerry's Rogue Jets also operates its jet boat excursions out of the port.
- Port of Brookings Harbor: this port is located at the mouth of the Chetco River and has the safest bar on the west coast (280 passable days per year). It is classified as a shallow-draft harbor, with 14 feet or less depth. The port supports recreational and commercial fishing, which is available most of the year, and is the busiest recreational port on the Oregon Coast. Port amenities include a full-service marina, a six-lane launch ramp and travel-lift services, full services for commercial fishing, docks and moorage facilities, and others. More than 5,000 commercial fishing vessels visit the port each year and more than 20 million pounds of bait, fuel, ice, and fish products cross its docks annually.

As noted in Technical Memorandum #2 (Policy Review), the 2017 Oregon Freight Plan (OFP) defines a statewide strategic freight network, including ports, but facilities in Curry County are not part of this strategic network.

Water facilities should be considered for their role in the transportation network both as facilities that may experience significant impacts from a natural disaster (e.g., tsunami) but also as potential alternate modes and "routes" following a natural disaster (e.g., landslides, etc.).

Pipelines

No known pipelines are present in Curry County.

Air, Water, and Pipeline Findings

- Three general aviation public airports serve Curry County: the Brookings Airport (Category IV airport recommended to be Category III), the Gold Beach Municipal Airport (Category IV airport), and the Cape Blanco State Airport (Category V airport recommended to be Category IV). Del Norte Airport in Crescent City, California also supports populations in the county.
- The Brookings Airport and the Cape Blanco State Airport are classified as Tier 3 and Tier 1 airports, respectively, in the Oregon Aviation Plan (OAP). These tiers reflect their capabilities and for making future investments, based on the forecast needs of their surrounding population and their provision of economic and commercial restoration to the entire region after a disaster event. These airports are also classified as Tier 2 airports in the Oregon Resilience Plan (ORP), which is a tier that represents a larger network of airports (compared to a more limited number of Tier 1 airports) that provide access to most rural areas and will be needed to restore major commercial operations following a natural disaster.
- The Brookings Airport has a “severe” Cascadia Event Hazard ranking (one below the highest ranking of “violent”) and the Cape Blanco State Airport has a “violent” Cascadia Event Hazard ranking, but both airports are located outside of known tsunami hazard areas. The Gold Beach Airport has a “violent” Cascadia Event Hazard ranking and is located inside known tsunami hazard areas.
- Three ports are located in the county in Port Orford, Gold Beach, and Brookings that support recreational and commercial fishing and provide a variety of other services. These facilities are not defined as part of the statewide strategic freight network, but they should be considered for their role in the transportation network both as facilities that may experience significant impacts from a natural disaster (e.g., tsunami) but also as potential alternate modes and “routes” following a natural disaster (e.g., landslides, etc.).
- No known pipelines are present in Curry County.

TITLE VI AND ENVIRONMENTAL JUSTICE

Title VI of the Civil Rights Act of 1964 and associated authorities prohibit discrimination on the basis of race, color, national origin, income, gender, and age. As an important authority related to Title VI, the Federal Executive Order on Environmental Justice provides the following three guiding principles for programs and project receiving federal funding:

- To avoid, minimize, or mitigate disproportionately high and adverse human health and environmental effects, including social and economic effects, on minority populations and low-income populations.
- To ensure the full and fair participation by all potentially affected communities in the transportation decision-making process.
- To prevent the denial of, reduction in, or significant delay in the receipt of benefits by minority and low-income populations.

Because ODOT receives federal funding for its projects and programs, the agency established a Title VI program to address nondiscrimination in all of its functions, including transportation planning. ODOT's 2015 Guidelines for Addressing Title VI and Environmental Justice (EJ) in Transportation Planning guides project managers to consider and include Title VI and EJ populations into scoping, public involvement analysis and outreach, solution alternatives analysis, and reporting. Within these four phases, specific steps include:

- Identify key destinations, transportation facilities, and services used by Title VI and EJ populations in the study area.
- Identify gaps in facilities and services or any other issues that may affect Title VI and EJ populations.
- Identify impacts and benefits of transportation improvements for Title VI and EJ populations when developing alternatives.
- Develop and use evaluation criteria addressing impacts and/or benefits to Title VI and EJ populations in determining recommended projects and/or preferred alternative.

In order to conduct these activities, populations protected by Title VI and related authorities must first be identified. Mapping the 2021 American Community Survey (ACS) 5-Year Estimates and 2020 Decennial Census data helped identify these populations. Maps of these populations and associated narrative are provided in the following sections.

Minority Groups⁴

Information on minority groups includes a combination of Hispanic or Latino origins as well as race at the Census Block geographic levels. Origin can be viewed as the heritage, nationality group, lineage, or country of birth of the person or the persons parents or ancestors before their arrival in the US. People who identify their origin as Hispanic or Latino may be any race. Race is based on racial classifications issued by the Office of Management and Budget (white, black or African American, American Indian or Alaska Native, Asian, Native Hawaiian or Other Pacific Islander, and Some Other Race). Respondents can select two or more races.

⁴ Information for selected demographic groups is from the 2020 Decennial Census data. Decennial Census data provides a full count of US population and various demographic information. Starting in 2010, the Census stopped using the long form survey and reports information on population, households, age, sex, race and ethnicity, and housing characteristics only. Additional information for other demographic and socioeconomic data was gathered using the American Community Survey.

Table 16 summarizes the minority groups for the State of Oregon and Curry County. Compared to the state, Curry County has an overall lower share of minority groups; approximately 28 percent of the overall state is of a minority race or ethnicity, compared to around 17 percent of the county.

For the purposes of showing minority groups in Figure 18, minority groups are considered a combination of the following individual classifications:

- Not Hispanic or Latino: Black or African American alone
- Not Hispanic or Latino: American Indian and Alaska Native alone
- Not Hispanic or Latino: Asian alone
- Not Hispanic or Latino: Native Hawaiian and Other Pacific Islander alone
- Not Hispanic or Latino: Some Other Race alone
- Not Hispanic or Latino: Two or More Races
- Hispanic or Latino

The highest concentration of minority groups in Curry County is within the Brookings UGB.

Table 16. Hispanic or Latino, and Not Hispanic or Latino by Race

	Oregon		Curry County	
Total:	4,237,256	Percent	23,446	Percent
Hispanic or Latino	588,757	13.89%	1,674	7.14%
Not Hispanic or Latino:	3,648,499	86.11%	21,772	92.86%
Population of one race:	3,389,814	80.00%	20,330	86.71%
White alone	3,036,158	71.65%	19,428	82.86%
Black or African American alone	78,658	1.86%	77	0.33%
American Indian and Alaska Native alone	42,042	0.99%	461	1.97%
Asian alone	191,797	4.53%	214	0.91%
Native Hawaiian and Other Pacific Islander alone	18,197	0.43%	25	0.11%
Some other race alone	22,962	0.54%	125	0.53%
Population of two or more races:	258,685	6.11%	1,442	6.15%

Source: Table P2, 2020 Decennial Census

Age⁵

Data on age is derived from a two-part census question (age and date of birth). Both age and date of birth is used in combination to determine the most accurate age as of the census reference date. Age data are tabulated in age groupings including populations 65 and older (Seniors) and populations 17 and younger (Youth).

As summarized in Table 17, youth populations comprise of approximately 15 percent of the overall county population. Youth populations in Curry County and its cities and Census Designated Places (CDPs) are generally lower than the overall portion of youth in the state. Curry County has a higher portion of senior population, 34 percent, compared to the state as a whole with about 19 percent.

⁵ Ibid

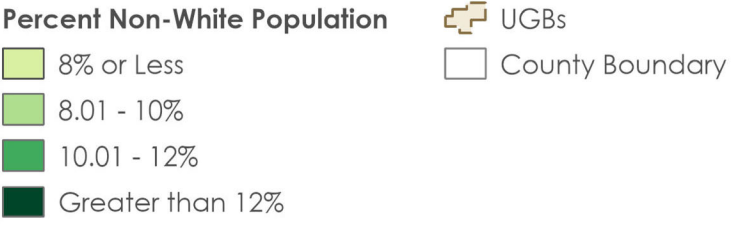
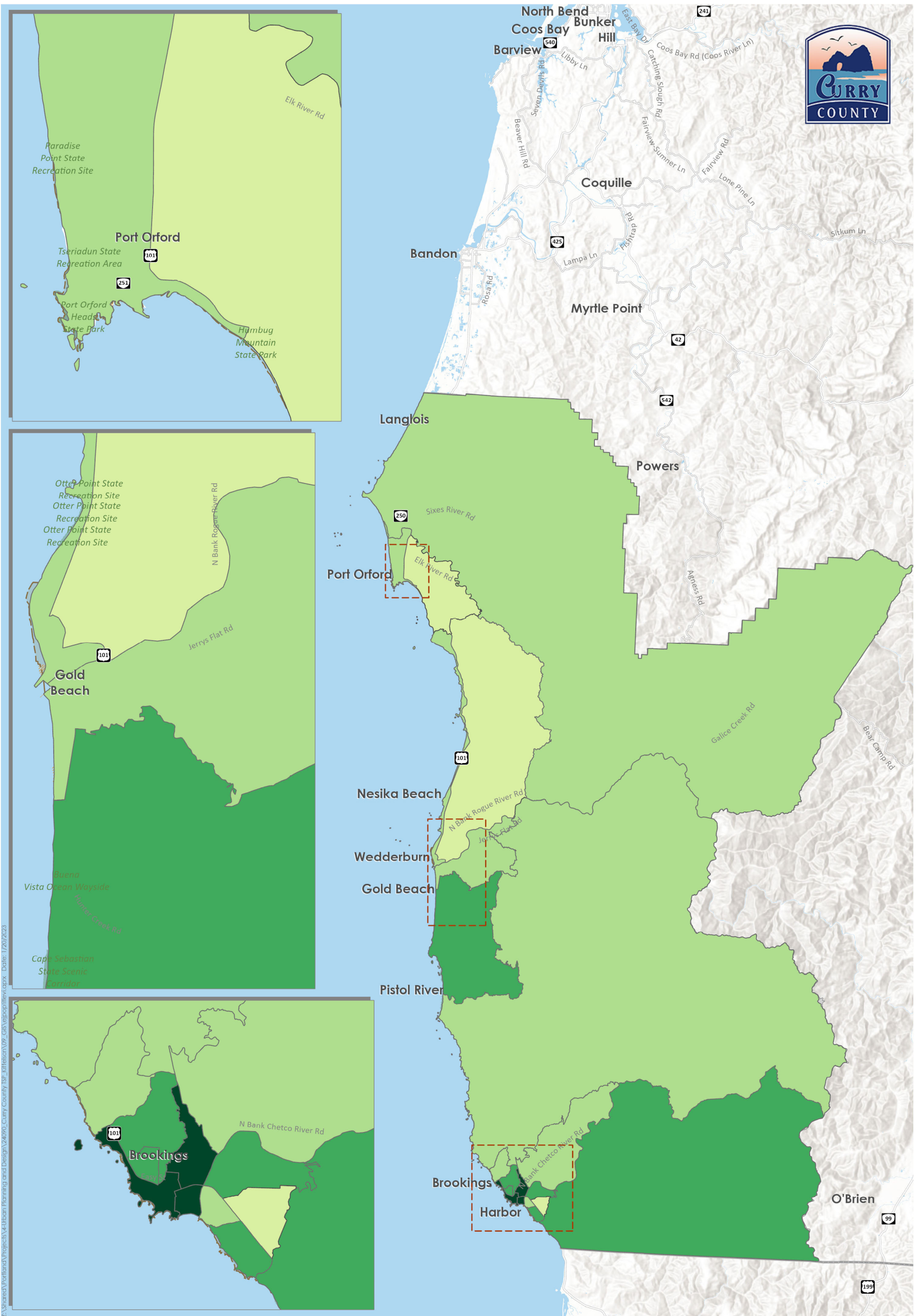


Figure 18
**Minority Populations
 Curry County, Oregon**

Table 17. Youth and Senior Populations

	Population	Youth	Percent	Seniors	Percent
State	4,246,155	858,666	20.22%	789,896	18.60%
County	23,234	3,332	14.34%	7,911	34.05%
City	10,194	1,175	11.53%	2,852	27.98%
Brookings city, Oregon	6,706	812	12.11%	1,811	27.01%
Gold Beach city, Oregon	2,615	343	13.12%	654	25.01%
Port Orford city, Oregon	873	20	2.29%	387	44.33%
Census Designated Place (CDP)	3,018	141	4.67%	1,907	63.19%
Harbor CDP, Oregon	1,957	18	0.92%	1,489	76.09%
Langlois CDP, Oregon	347	46	13.26%	169	48.70%
Nesika Beach CDP, Oregon	331	0	0.00%	137	41.39%
Wedderburn CDP, Oregon	383	77	20.10%	112	29.24%

Source: Table B01001, 2021 5-year American Community Survey Estimates Detailed Tables

The location of elderly and youth populations is provided at the Census Block geographic level in Figure 19 and Figure 20. The highest concentration of seniors is south of Harbor where over 75% of the population is age 65 or older. The highest concentration of youth population is in Wedderburn, just north of Gold Beach where approximately 20% of the population is age 17 and younger.

Low Income⁶

The federal poverty threshold is calculated by the size of the household and is adjusted annually. In 2021 the threshold for an individual is \$12,880 in annual earnings, and \$26,500 for a household of four.⁷ The US Census Bureau measures poverty by looking at the ratio between a household's income and the household's poverty threshold, called the Ratio of Income to Poverty. Households with an Income to Poverty Ratio below 1 are eligible for federal assistance programs; however, households with a ratio between 1 and 2 still experience the impacts of poverty and may be eligible for other benefits, such as the Supplemental Nutrition Assistance Program (SNAP, formerly known as Food Stamps).

Figure 21 displays the percentage of the population in Curry County with a Poverty to Income Ratio below 2. As shown in Figure 21, the Census Block Group in the northeast corner of the county and the Census Block Groups southeast of Brookings have the highest percentage of households with a poverty ratio below 2. Port Orford has the highest percentage of households with a poverty ratio below 2 out of the three incorporated cities in the county.

As shown in Table 18, compared to the overall State of Oregon, a similar portion of the overall population in Curry County is in poverty with a ratio of income to poverty below 1. Approximately 13 percent of the population in Curry County is in poverty, compared to approximately 12 percent of the overall State.

⁶ MIG compiled information for selected demographic groups using American Community Survey (ACS) data. ACS data was used where 2020 decennial information on the identified demographics was not available. The ACS is a nation-wide survey conducted by the Census Bureau to provide estimates on population, housing unit, and household characteristics at the Census Block Group geographic level. Because the ACS is based on a sample, they have a margin of error. The margin of error, combined with the ACS estimate, provides a range of values within which the actual value may fall. As such, demographic information shown below should be considered carefully.

⁷ <https://www.healthcare.gov/glossary/federal-poverty-level-fpl/>

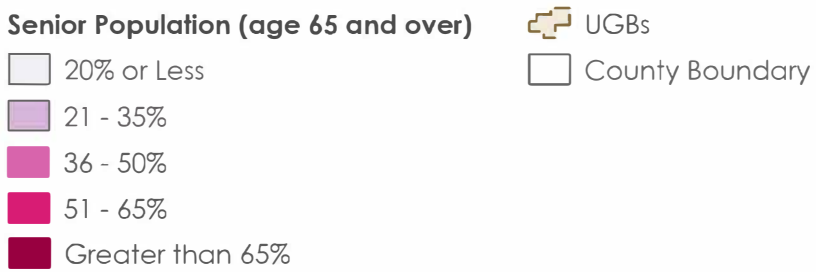
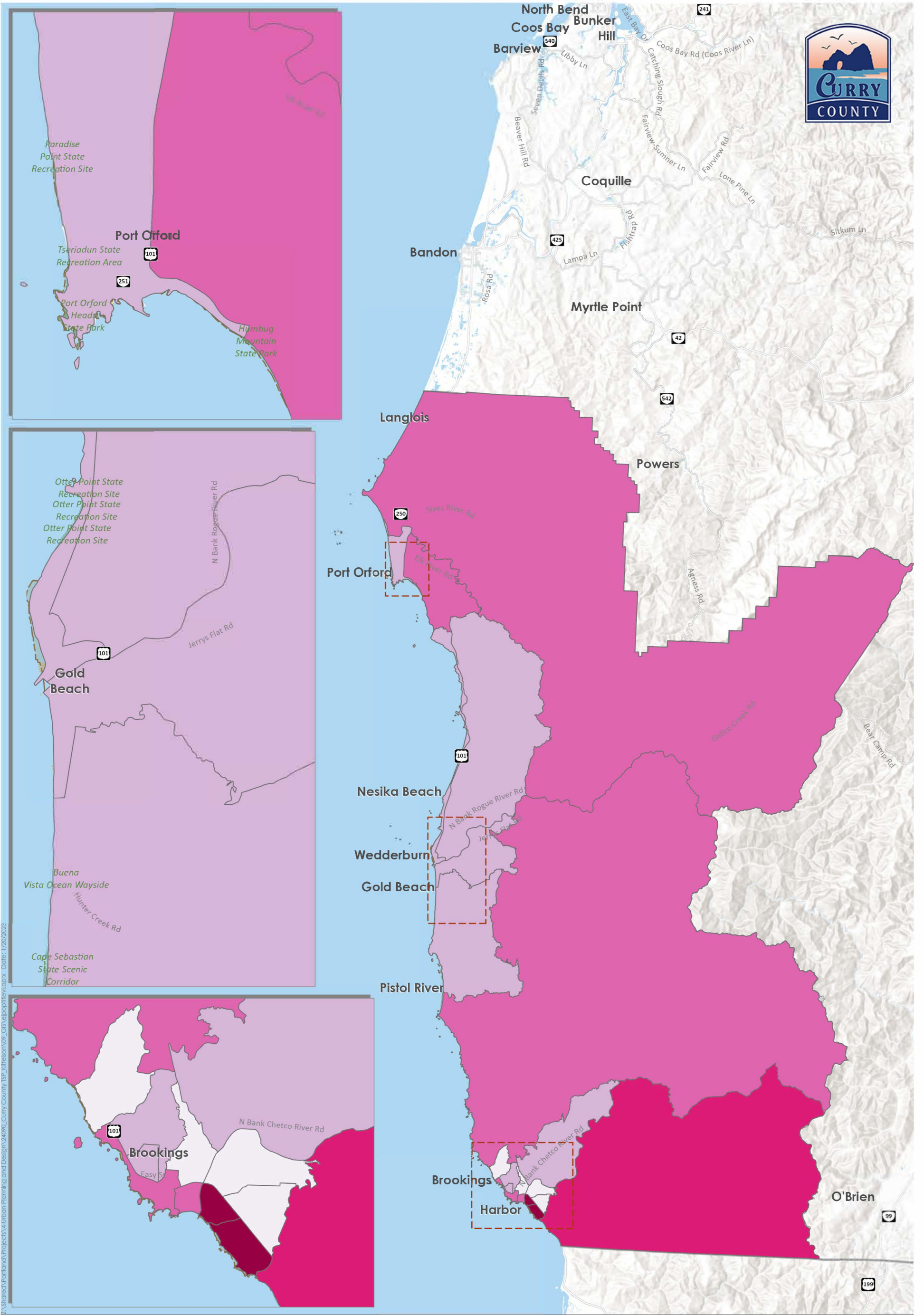


Figure 19
Senior Population
 Curry County, Oregon

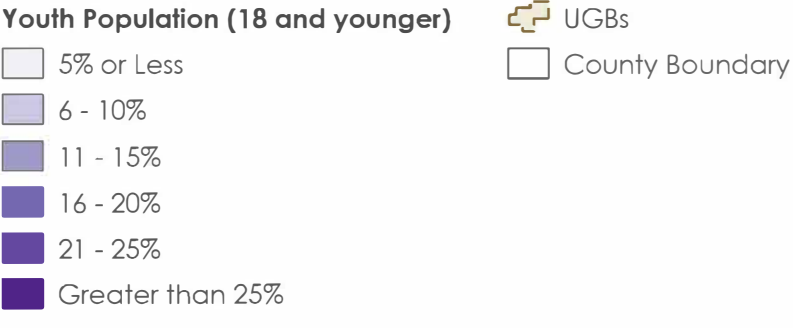
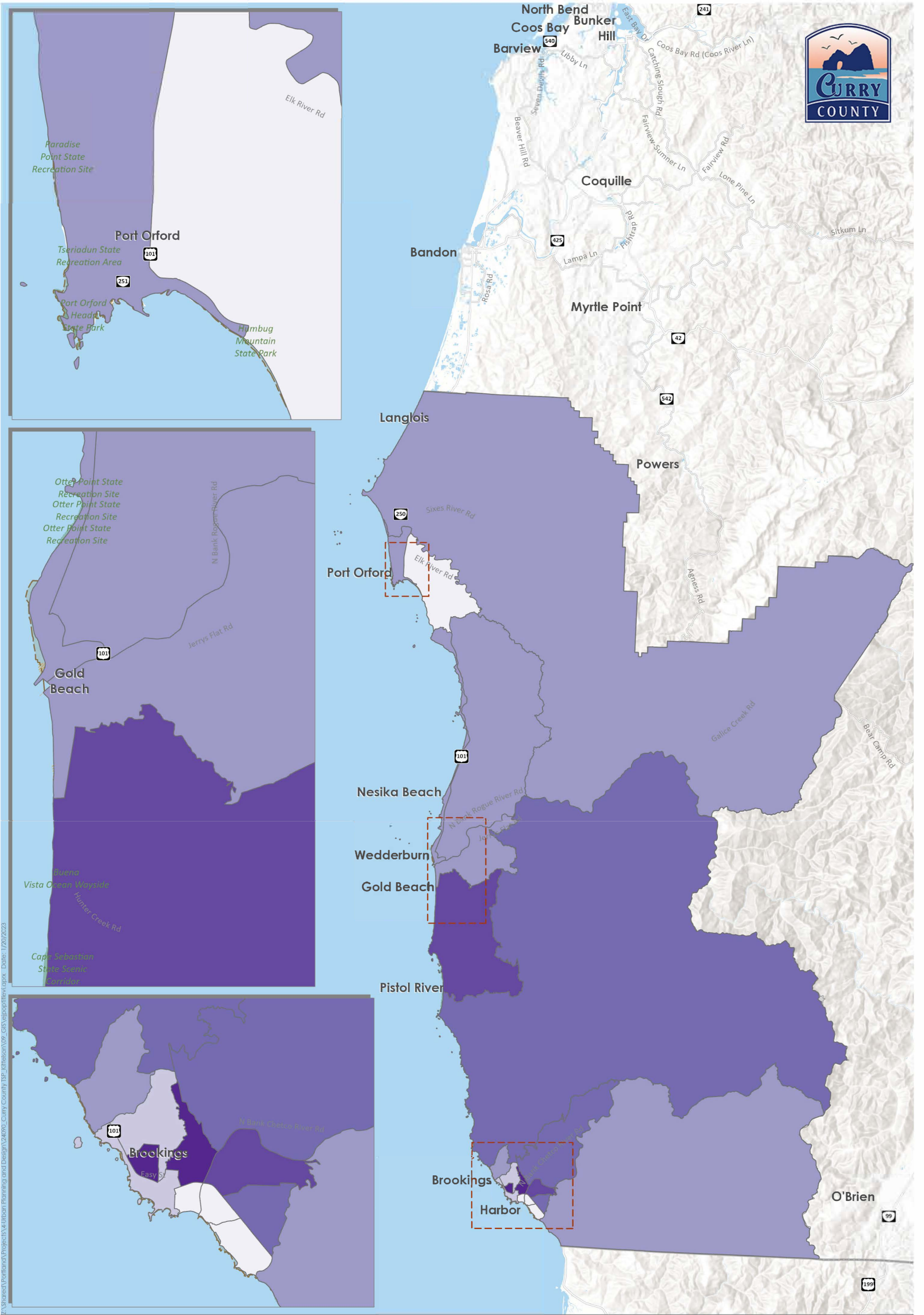
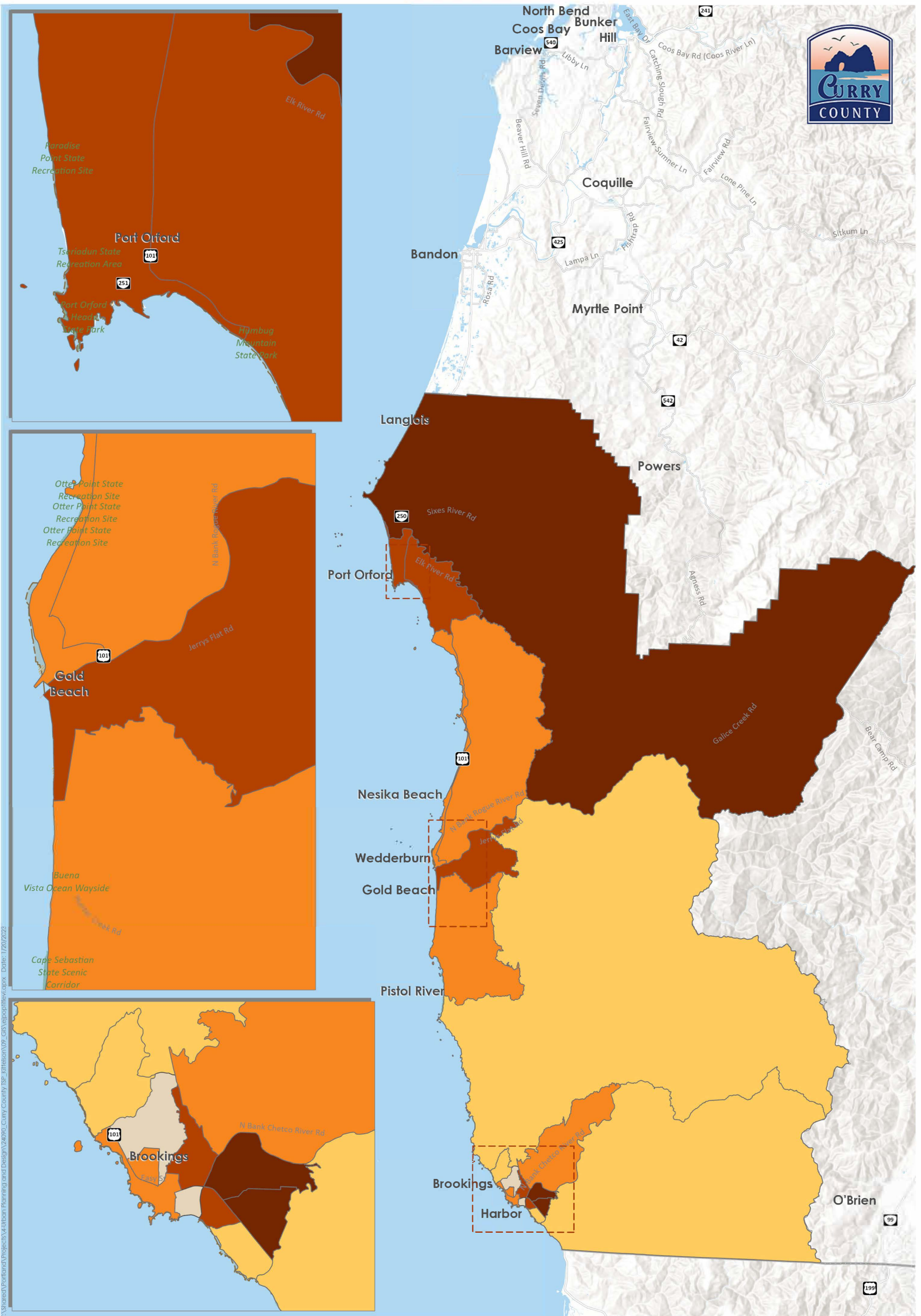


Figure 20
Youth Population
 Curry County, Oregon



Percentage of Households with Poverty Ratio Below 2

- 20% or Less
- 21 - 30%
- 31 - 40%
- 41 - 50%
- Greater than 50%

UGBs
County Boundary

0 5 10 Miles

Figure 21
Households Experiencing Poverty
Curry County, Oregon

Table 18. Ratio of Income to Poverty

	Oregon		Curry County	
	Population	Percent	Population	Percent
Total:	4,128,333	--	23,084	--
Under .50	228,136	5.53%	843	3.65%
.50 to .99	270,381	6.55%	2,203	9.54%
1.00 to 1.24	165,091	4.00%	1,081	4.68%
1.25 to 1.49	173,155	4.19%	1,045	4.53%
1.50 to 1.84	239,193	5.79%	2,456	10.64%
1.85 to 1.99	107,415	2.60%	537	2.33%
2.00 and over	2,944,962	71.34%	14,919	64.63%

Source: C17002: Ratio of Income to Poverty in the Past 12 Months, 2021 5-year American Community Survey

Title VI and Environmental Justice Findings

- 17% of Curry County's population is of a minority race or ethnicity (as compared to 28% of the overall states population). The highest concentration of these groups is within the Brookings UGB.
- Curry County has a higher portion of senior population, 34%, compared to the state as a whole with about 19%. The highest concentration of seniors is south of Harbor where over 75% of the population is age 65 or older.
- Approximately 13% of the population in Curry County is in poverty, compared to approximately 12% of the overall State. The Census Block Group in the northeast corner of the county and the Census Block Group southeast of Brookings have the highest percentage of households with a poverty ratio below 2. Port Orford has the highest percentage of households with a poverty ratio below 2 out of the three incorporated cities in the county.

ENVIRONMENTAL CONSTRAINTS

This section summarizes environmental information related to State planning Goal 5; Congestion Mitigation and Air Quality Improvement (CMAQ); Federal Emergency Management Agency (FEMA); the National Heritage Database; State Historic Preservation Office (SHPO); local historic societies; threatened and endangered species; Section 4(f) of the U.S. Department of Transportation Act; Section 6(f) of the Land and Water Conservation Act; wetland areas; wildlife areas; and tribal lands.

Goal 5

State Planning Goal 5 requires that the County develop and adopt programs that would ensure open spaces, protect scenic and historic areas and natural resources, and promote a healthy and visually attractive environment in harmony with the natural landscape character. Goal 5 identifies twelve resources to which the goal is applied: 1) Open space; 2) mineral and aggregate resources; 3) energy sources; 4) fish and wildlife areas and habitats; 5) ecologically significant natural areas; 6) outstanding scenic views and sites; 7) water resources; 8) wilderness;

9) historic resources; 10) cultural areas; 11) Oregon recreation trails; and, 12) federal wild and scenic waterways and state scenic waterways.

Curry County is rich in these resources; as a result, these Goal 5 resources should be identified when planning projects and avoidance measures employed during the planning and design phase to limit impacts to these resources to the greatest extent practicable. It is advised to refer to the Comprehensive Plan and County planning officials when developing projects to verify Goal 5 resources are avoided or minimized during project development. Impacts to Goal 5 resources will likely require local land use permits.

Federal Emergency Management Agency

The Federal Emergency Management Agency (FEMA) asserts jurisdiction over all floodplains and floodways. There are several FEMA mapped floodways and floodplains within Curry County. Any project that will occur in a floodway or floodplain will require approval from FEMA via a floodplain permit application. FEMA has given authority to local agencies to review and approve floodplain applications, so any project that could result in fill within the floodplain or floodway will require coordination with the local agency planning department. Projects requiring fill within the floodway will be required to ensure there is no rise in the flood elevation as a result of the project, and projects with fill in the floodplain must not cause more than one foot of rise to the flood elevation. Projects adding fill within floodplains and floodways should be avoided if possible; if avoidance is not practical, fill impacts should be mitigated.

Tsunami Inundation Zone

Figure 22 shows the tsunami inundation zones for Port Orford, Gold Beach, and Brookings within Curry County. The tsunami inundation zone is the distance inland that a tsunami is predicted to reach. It is important for developed areas in a tsunami inundation zone to consider potential impacts of a tsunami, such as catastrophic flooding, on the urban area. The transportation networks of areas within a tsunami inundation zone should also account for safe evacuation routes and routes to tsunami refuge areas.

National Heritage Database

According to the Oregon Historic Sites Database, Curry County is home to 152 historic resources listed on the National Register, which include buildings, districts, objects, sites, and structures. A vast majority of these eligible and significant resources are located in Port Orford and its vicinity. Other resource locations within Curry County include Brookings, Carpenterville vicinity, Pistol River vicinity, and Sixes and its vicinity.

State Historic Preservation Office

There are over 150 historic resources within Curry County per the State Historic Preservation Office (SHPO). One-hundred five of these resources are considered eligible or significant. Transportation projects with the potential to impact any resource over 50 years old should work with SHPO to ensure the project does not impact any resources potentially eligible for listing on the national register of historic places. It's important to note that ditches, walls, culverts, bridges, etc., currently contained within the transportation system could be eligible for listing, so even projects that don't impact structures should be coordinated with SHPO to verify potentially eligible resources are not impacted as a result of transportation projects.

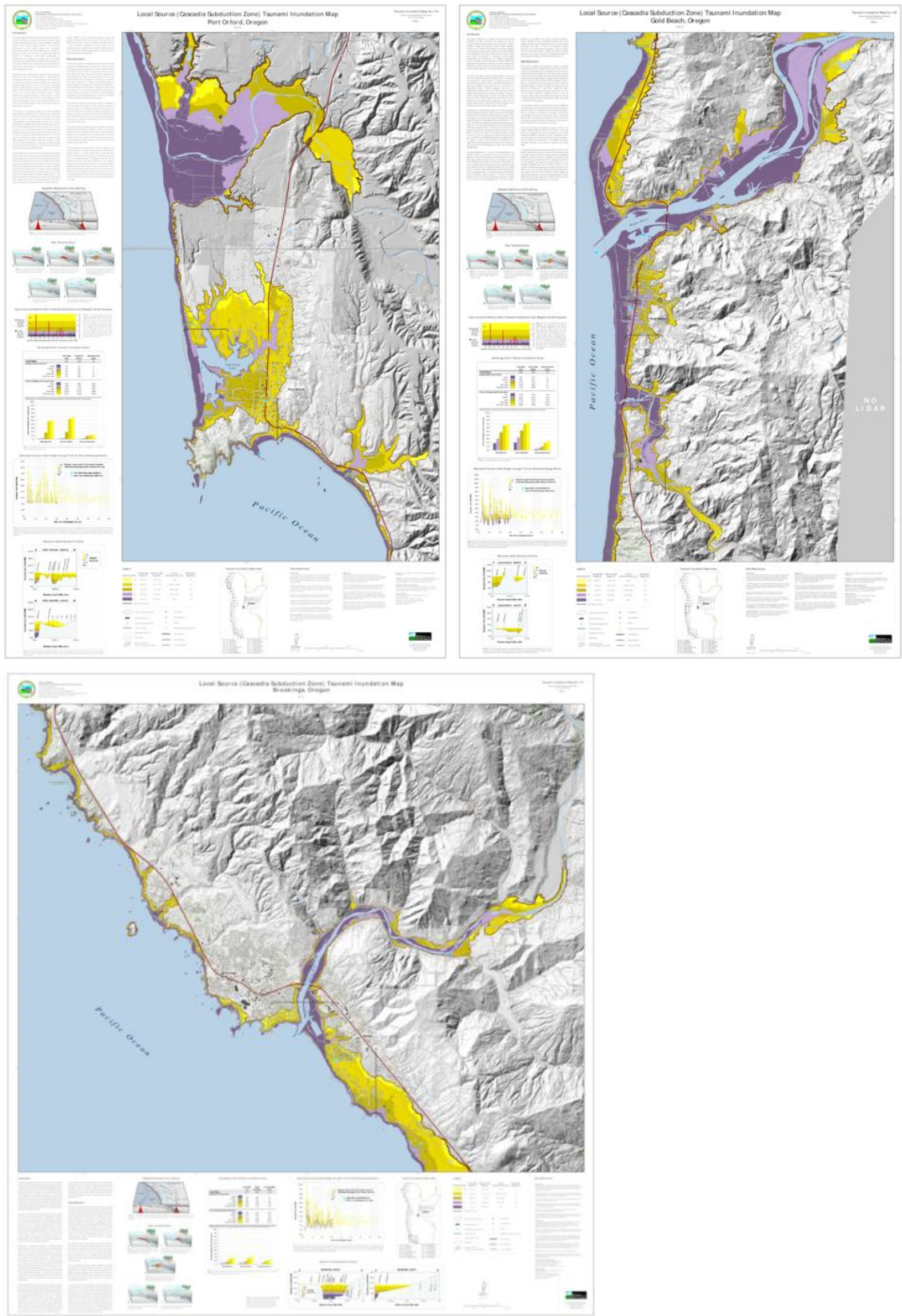


Figure 22. Tsunami Inundation Zones (Gold Beach, Brookings, Port Orford)

Threatened and Endangered Species

There are currently numerous plant and animal species present in Curry County that are federally listed as either threatened or endangered under the federal endangered species act. Additionally, Curry County contains critical habitat as well as state listed species, species of concern, and sensitive species. Transportation projects that impact undisturbed ground, remove trees or shrubs, change drainage patterns, cross waterways, or result in stormwater runoff should coordinate with the Oregon Department of Fish and Wildlife, the US Fish and Wildlife Service, National Marine Fisheries Service, and the Oregon Department of Agriculture to determine if endangered, threatened, or sensitive species and/or their habitat could be impacted as a result of the project. Designing projects to avoid impacting endangered, threatened, or sensitive species and their habitat should be a priority.

Section 4(f) and 6(f)

Section 4(f) of the U.S. Department of Transportation Act prohibits projects with Federal Transit Administration funds from using land from publicly owned parks, recreation areas (including recreational trails), wildlife and waterfowl refuges, or public and private historic properties, unless there is no feasible and prudent alternative. The Section 4(f) process is arduous and costly, so projects impacting 4(f) properties should be avoided or minimized.

Section 6(f) of the Land and Water Conservation Act requires that the conversion of lands or facilities acquired with Land and Water Conservation Act funds be coordinated with the Department of Interior. During the project planning process, if temporary or permanent right-of-way will be required, the project proponent should search the land and water conservation fund database to determine if lands encumbered with land and water conservation funds will be impacted. It is best to avoid impacting these properties as the process for mitigating Section 6(f) property impacts is cumbersome, expensive, and requires a long approval process.

Wetland Areas

Impacts to wetlands require prior approval from both the Oregon Department of State Lands (DSL) and the US Army Corps of Engineers (USACE). Projects requiring impacts to unimproved services should be delineated by a wetland professional to determine if wetlands are present. If they are determined to be present by DSL and the USACE, a Joint Permit Application will be required. DSL and the USACE must review the Joint Permit Application and wetland mitigation will be required before the agencies approve the permit to impact wetlands.

Wildlife Areas

Curry County has large areas of undeveloped land and a checkerboard of public lands that provide a range of habitats to support wildlife. During the project planning process, adjacent State, Federal, and local landowners should be consulted to determine if the project passes through any known (official and unofficial) wildlife areas. Projects in wildlife usage areas should be planned carefully to avoid impacting wildlife movement and habitat, as well as to minimize interactions with the traveling public.

Known Hazmat Sites

Although there are no known superfund sites in Curry County, there are hundreds of known contaminated sites and spills that have occurred throughout the county. During project development, especially when right-of-way is required, a hazardous materials study should be completed to determine if contamination exists within the project location. Areas adjacent to roads and historically industrialized and agricultural areas are particularly suspect for potential

contamination. Hazardous materials studies can assess the level of contamination present as well as recommend worker safety practices to be utilized during construction.

Tribal Lands

Curry County is located on the ancestral lands of the Cayuse, Umatilla, and Walla Walla tribes; Tolowa Dee-ni'; Chit-dee-ni (Chetco); Coquille Indian Tribe; and the Confederated Tribes of the Siletz Indians. The Coquille Indian Tribe upholds tribal government, protects and preserves tribal sovereignty, history, culture, and the general welfare of the tribal membership, and serves to provide for the long-term economic needs of the tribe and its members through economic development of tribal lands. The Tribe also holds ownership of and manages the land of the Coquille Tribal Forest, the Sek-wet-se Tribal Forest, and the Empire Reservation.

Environmental Constraint Findings

- Curry County is rich in Goal 5 resources.
- Several FEMA mapped floodways and floodplains are within Curry County.
- Curry County is home to 152 historic resources listed on the National Register, mostly located in and around Port Orford.
- Over 150 historic resources are located within Curry County per SHPO and 105 are considered eligible or significant.
- Numerous plant and animal species present in Curry County are federally listed as either threatened or endangered under the federal endangered species act. Curry County also contains critical habitat as well as state listed species, species of concern, and sensitive species.
- Curry County has large areas of undeveloped land and a checkerboard of public lands that provide a range of habitats to support wildlife.
- Although there are no known superfund sites in Curry County, there are hundreds of known contaminated sites and spills that have occurred throughout the county.
- Curry County is located on the ancestral lands of the Cayuse, Umatilla, and Walla Walla tribes; Tolowa Dee-ni'; Chit-dee-ni (Chetco); Coquille Indian Tribe; and the Confederated Tribes of the Siletz Indians.

REFERENCES

1. Oregon Department of Land Conservation and Development. *Oregon Transportation Planning Rule, 2022*.
2. Oregon Department of Transportation. *Oregon Highway Plan, 2015*.
3. Curry County. *Comprehensive Plan, 2009*.
4. Oregon Seismic Safety Policy Advisory Commission. *Oregon Resilience Plan, 2013*.
5. Oregon Department of Transportation. *Highway Design Manual, 2023*.
6. Oregon Department of Aviation. *Oregon Aviation Plan 6.0, 2018*.

ATTACHMENT A – FEDERAL AND COUNTY FUNCTIONAL CLASSIFICATIONS OF COUNTY ROADS

Road	From	To	County Classification	Federal Classification
AGNESS-ILLAHE RD	920 FT S. OF COUGER LN	USFS ROAD #33	RMiC - Rural Minor Collector	None
AGNESS-ILLAHE RD	USFS RD #33	100FT PAST FOSTER BAR RD	RMiC - Rural Minor Collector	None
AGNESS-ILLAHE RD	100FT PAST FOSTER BAR RD	END OF PAVEMENT	RMiC - Rural Minor Collector	None
AGNESS-ILLAHE RD	END OF PAVEMENT	END COUNTY ROAD	RMiC - Rural Minor Collector	None
AIRPORT RD	ST. HWY. 101	PARKING LOT	RMaC - Rural Major Collector	Major Collector
BOWMAN ST	LANGLOIS MTN RD	1ST STREET	R - Residential/Local	Minor Collector
CAPE FERRELO RD	ST. HWY. 101	CARPENTERVILLE RD	RMaC - Rural Major Collector	Major Collector
CEDAR VALLEY RD	HOUSE #34205	SIDNEY WY	RMaC - Rural Major Collector	Major Collector
CEDAR VALLEY RD	MILE POST 6.177	MILE POST 7.8	RMaC - Rural Major Collector	Major Collector
CEDAR VALLEY RD	MILE POST 7.8	N. BANK ROGUE RIVER RD	RMaC - Rural Major Collector	Major Collector
CEDAR VALLEY RD	SIDNEY WY	MILE POST 6.177	RMaC - Rural Major Collector	Major Collector
CEDAR VALLEY RD	OPHIR RD	HOUSE #34205	RMaC - Rural Major Collector	Major Collector
CEMETERY LOOP RD	ST. HWY. 101 (SOUTH END)	DEADY ST (PORT ORFORD)	RMiC - Rural Minor Collector	None
CHINA MOUNTAIN RD	NOBLE DR	END OF PAVEMENT	RMiC - Rural Minor Collector	None
CHINA MOUNTAIN RD	END OF PAVEMENT	HUMBUG PARK ROAD	RMiC - Rural Minor Collector	None
CHINA MOUNTAIN RD	NOBLE DR	END OF PAVEMENT	RMiC - Rural Minor Collector	None
E. BENHAM LN	ST. HWY 101	ROAD NARROWS #98295	RMiC - Rural Minor Collector	None
EDSON CREEK RD	U.S. HWY. 101	N. BANK ROGUE RIVER RD	RMaC - Rural Major Collector	Major Collector
ELK RIVER RD	MP 5.58	USFS BOUNDARY	RMaC - Rural Major Collector	Minor Collector
ELK RIVER RD	ELK RIVER FIRE STATION	MP 5.58	RMaC - Rural Major Collector	Major Collector
ELK RIVER RD	ST. HWY. 101	ELK RIVER FIRE STATION	RMaC - Rural Major Collector	Major Collector
EUCHRE CREEK RD	OPHIR RD	BRIDGE	RMaC - Rural Major Collector	Major Collector
FLORAS CREEK RD	MILE POST 2.61/ROAD NARROWS	END OF AC OVERLAY/MP 3.21	RMaC - Rural Major Collector	Major Collector

Road	From	To	County Classification	Federal Classification
FLORAS CREEK RD	ST. HWY. 101	MILE POST 2.61/ ROAD NARROWS	RMaC - Rural Major Collector	Major Collector
FLORAS CREEK RD	MP 3.21/ END OF AC OVERLAY	END OF PAVEMENT	RMaC - Rural Major Collector	Major Collector
FLORAS CREEK RD	BEG. OF AC W. OF BRIDGE	END OF AC E. OF BRIDGE	RMaC - Rural Major Collector	Major Collector
FLORAS CREEK RD	END AC E. OF BRIDGE	END OF MORRIL BRIDGE	RMaC - Rural Major Collector	Major Collector
FLORAS CREEK RD	END PAVEMENT	BEG AC W. OF BRIDGE	RMaC - Rural Major Collector	Major Collector
FLORAS LAKE LP	ST. HWY. 101 - NORTH END	ST. HWY. 101 - SOUTH END	RMaC - Rural Major Collector	Major Collector
FLORAS LAKE RD	FLORAS LAKE LP.	LAKES END DR	RMiC - Rural Minor Collector	Minor Collector
GARDNER RIDGE RD	N. BANK CHETCO RIV. RD.	END OF 1996 OVERLAY, MP 3.43	RMiC - Rural Minor Collector	Minor Collector
GARDNER RIDGE RD	MP 8.43	90 FT PAST HOUSE #00595	RMiC - Rural Minor Collector	Minor Collector
GARDNER RIDGE RD	90 FT PAST HOUSE #00595	CATTLE GUARD	RMiC - Rural Minor Collector	Minor Collector
GARDNER RIDGE RD	END OF 1996 OVERLAY, MP-3.43	USFS RD 39-13-25.0	RMiC - Rural Minor Collector	Minor Collector
GARDNER RIDGE RD	USFS RD 39-13-25.0	MP 8.43	RMiC - Rural Minor Collector	Minor Collector
GRASSY KNOB RD	MILE POST 2	END OF PAVEMENT	RMiC - Rural Minor Collector	None
GRASSY KNOB RD	U.S. HWY. 101	MILE POST 2	RMiC - Rural Minor Collector	None
GRIZZLY MOUNTAIN RD	END OF PAVEMENT	END COUNTY ROAD SIGN	RMiC - Rural Minor Collector	None
GRIZZLY MOUNTAIN RD	BEG PAVE 1500ft E. OF BEAR MTN	END OF PAVEMENT	RMiC - Rural Minor Collector	None
GRIZZLY MOUNTAIN RD	CITY LIMITS - BEAR MTN RD	CATTLEGUARD	RMiC - Rural Minor Collector	None
HUNTER CREEK LP	ST. HWY. 101 (NORTH END)	HUNTER CREEK RD	RMaC - Rural Major Collector	Major Collector
HUNTER CREEK RD	COUNTY SHOPS/GUARD RAIL	END OF PAVEMENT	RMaC - Rural Major Collector	Major Collector
HUNTER CREEK RD	ST. HWY. 101	COUNTY SHOPS/GUARD RAIL	RMaC - Rural Major Collector	Major Collector
HUNTER CREEK RD	COUNTY SHOPS/GUARD RAIL	END OF PAVEMENT	RMaC - Rural Major Collector	Major Collector
JERRYS FLAT RD	SAUNDERS CREEK BRIDGE	OLD MILL SITE	RMiA - Rural Minor Arterial	Minor Arterial
JERRYS FLAT RD	OLD MILL SITE	HUNTLEY PARK CAMPGROUND	RMiA - Rural Minor Arterial	Minor Arterial
JERRYS FLAT RD	HOUSE #94819	VISTA LP	RMiA - Rural Minor Arterial	Minor Arterial
JERRYS FLAT RD	650' E. OF ST. HWY. 101	HOUSE #94819	RMiA - Rural Minor Arterial	Minor Arterial
JERRYS FLAT RD	HUNTLEY PARK CAMPGROUND	USFS BOUNDARY	RMiA - Rural Minor Arterial	Minor Arterial
JERRYS FLAT RD	VISTA LP	SAUNDERS CREEK BRIDGE	RMiA - Rural Minor Arterial	Minor Arterial

Road	From	To	County Classification	Federal Classification
JERRYS FLAT RD	OLD MILL SITE	HUNTLEY PARK CAMPGROUND	RMiA - Rural Minor Arterial	Minor Arterial
LANGLOIS MTN RD	ST. HWY. 101	MILE POST 1.799/OVERLAY	RMiC - Rural Minor Collector	Minor Collector
LANGLOIS MTN RD	MILE POST 1.799/OVERLAY	ROAD NARROWS	RMiC - Rural Minor Collector	Minor Collector
LANGLOIS MTN RD	ROAD NARROWS	MILE POST 6.0	RMiC - Rural Minor Collector	Minor Collector
LANGLOIS MTN RD	MILE POST 8.26/PENETRATION	END OF COUNTY ROAD	RMiC - Rural Minor Collector	Minor Collector
LANGLOIS MTN RD	MILE POST 6.0	MILE POST 8.26/PENETRATION	RMiC - Rural Minor Collector	Minor Collector
LITTLE SOUTH FORK RD	HUNTER CR. RD.	END COUNTY ROAD SIGN	RMaC - Rural Major Collector	Major Collector
LOWER HARBOR RD	SHOPPING CENTER AVE	BOAT BASIN RD	RMaC - Rural Major Collector	Major Collector
LOWER HARBOR RD	ST. HWY. 101	SHOPPING CENTER AVE	RMaC - Rural Major Collector	Major Collector
N BANK CHETCO RIVER RD	MOUNTAIN DR	DON CAMERON BRIDGE	RMaC - Rural Major Collector	Major Collector
N BANK CHETCO RIVER RD	200' S. OF #97656/CITY LIMITS	MOUNTAIN DR	RMaC - Rural Major Collector	Major Collector
N BANK CHETCO RIVER RD	DON CAMERON BRIDGE	DONALDS RD	RMiC - Rural Minor Collector	Major Collector
N BANK CHETCO RIVER RD	DONALDS RD	USFS BOUNDARY	RMiC - Rural Minor Collector	Major Collector
N BANK CHETCO RIVER RD	MOUNTAIN DR	DON CAMERON BRIDGE	RMaC - Rural Major Collector	Major Collector
N BANK PISTOL RIVER RD	MILE POST 2.0	END OF PAVEMENT	RMiC - Rural Minor Collector	Minor Collector
N BANK PISTOL RIVER RD	PISTOL RIVER LP.	MILE POST 2.0	RMiC - Rural Minor Collector	Minor Collector
N BANK PISTOL RIVER RD	BEGIN BRIDGE PAVEMENT	END BRIDGE PAVEMENT	RMiC - Rural Minor Collector	Minor Collector
N BANK PISTOL RIVER RD	END PAVEMENT	START BRIDGE PAVEMENT	RMiC - Rural Minor Collector	Minor Collector
N BANK PISTOL RIVER RD	END BRIDGE PAVEMENT	END COUNTY ROAD SIGN CATTLE GD	RMiC - Rural Minor Collector	Minor Collector
N BANK ROGUE RIVER RD	ST. HWY. 101	MILE POST 2.22	RMaC - Rural Major Collector	Major Collector
N BANK ROGUE RIVER RD	MILLER RANCH RD	USFS ROAD	RMaC - Rural Major Collector	Major Collector
N BANK ROGUE RIVER RD	MILE POST 2.22	EDSON CREEK RD	RMaC - Rural Major Collector	Major Collector
N BANK ROGUE RIVER RD	EDSON CREEK	4 SEASONS RV RESORT	RMaC - Rural Major Collector	Major Collector
N BANK ROGUE RIVER RD	4 SEASONS RV RESORT	MILLER RANCH RD	RMaC - Rural Major Collector	Major Collector
N BANK ROGUE RIVER RD	ST. HWY. 101	MILE POST 2.22	RMaC - Rural Major Collector	Major Collector
NESIKA RD	ST. HWY. 101 (NORTH END)	ST. HWY. 101 (SOUTH END)	RMiC - Rural Minor Collector	Minor Collector
OAK FLAT RD	USFS RD.#33/AGNESS RD	END OF PAVEMENT	RMiC - Rural Minor Collector	Minor Collector

Road	From	To	County Classification	Federal Classification
OCEANVIEW DR	WENBOURNE LN	#15398/ BIKE PATH BEGINS	RMiC - Rural Minor Collector	Minor Collector
OCEANVIEW DR	W. BENHAM LN	WENBOURNE LN	RMiC - Rural Minor Collector	Minor Collector
OCEANVIEW DR	#15398/BIKE PATH BEGINS	ST. HWY. 101	RMiC - Rural Minor Collector	Minor Collector
OCEANVIEW DR	#15398/BIKE PATH BEGINS	ST. HWY. 101	RMiC - Rural Minor Collector	Minor Collector
OCEANVIEW DR	#15398/BIKE PATH BEGINS	ST. HWY. 101	RMiC - Rural Minor Collector	Minor Collector
OLD COAST RD	#30756 (CATTLE GUARD)	ST. HWY. 101 WEST SIDE	RMiC - Rural Minor Collector	None
OLD COAST RD	WEDDERBURN LOOP RD	#30756 (CATTLE GUARD)	RMiC - Rural Minor Collector	None
OLD COAST RD	BEGIN PAVEMENT	HWY 101 NORTH EAST ENTRANCE	RMiC - Rural Minor Collector	None
OLD COAST RD	ST. HWY. 101 EAST SIDE	BEGIN PAVEMENT	RMiC - Rural Minor Collector	None
OLD COUNTY RD	END PAVEMENT	END COUNTY ROAD / GATE	RMiC - Rural Minor Collector	None
OLD COUNTY RD	BROOKINGS C/L, #16901	END OF PAVEMENT	RMiC - Rural Minor Collector	None
OLD COUNTY RD	END PAVEMENT	END COUNTY ROAD / GATE	RMiC - Rural Minor Collector	None
OPHIR RD	ST. HWY. 101 (NORTH END)	MP 0.38	RMiC - Rural Minor Collector	Major Collector
OPHIR RD	MP 0.38	HONEY BEAR C.G. #34161	RMiC - Rural Minor Collector	Minor Collector
OPHIR RD	HONEY BEAR C.G. #34161	ST. HWY. 101 (SOUTH END)	RMiC - Rural Minor Collector	Minor Collector
PARADISE POINT RD	ST. HWY. 101	END OF PAVEMENT	R - Residential/Local	Minor Collector
PARKVIEW DR	130 FT EAST OF VISTA RIDGE DR	AIRPORT PARKING LOT	RMiC - Rural Minor Collector	None
PEDRIOLI DR	ST. HWY. 101	OCEANVIEW DR	RMiC - Rural Minor Collector	None
PEDRIOLI DR	OCEANVIEW DR	HOUSE #15500 / END OF ROAD	RMiC - Rural Minor Collector	None
PEDRIOLI DR	ST. HWY. 101	OCEANVIEW DR	RMiC - Rural Minor Collector	None
PISTOL RIVER LP	CAPE VIEW LP	CARPENTERVILLE RD	RMaC - Rural Major Collector	Major Collector
RAINBOW ROCK RD	AQUA VISTA LN	CARPENTERVILLE RD (NORTH END)	RMiC - Rural Minor Collector	None
RAINBOW ROCK RD	CARPENTERVILLE RD (SOUTH END)	AQUA VISTA LN	RMiC - Rural Minor Collector	None
RAINBOW ROCK RD	AQUA VISTA LN	CARPENTERVILLE RD (NORTH END)	RMiC - Rural Minor Collector	None
S BANK CHETCO RIVER RD	HARBOR VIEW CIR	CAMPBELL RD	RMaC - Rural Major Collector	Major Collector
S BANK CHETCO RIVER RD	CAMPBELL RD	MILL CREEK BRIDGE	RMaC - Rural Major Collector	Major Collector
S BANK CHETCO RIVER RD	S.B. CHETCO UNDER PASS RD.	HARBOR VIEW CIR.	RMaC - Rural Major Collector	Major Collector
S BANK CHETCO RIVER RD	MILL CREEK BRIDGE	CATTLE GUARD	RMaC - Rural Major Collector	Major Collector

Road	From	To	County Classification	Federal Classification
S BANK CHETCO RIVER RD	CAMPBELL RD	MILL CREEK BRIDGE	RMaC - Rural Major Collector	Major Collector
S BANK CHETCO UNDERPASS	LOWER HARBOR RD	S. BANK CHETCO RIVER RD	RMaC - Rural Major Collector	Major Collector
SECOND ST	BOWMAN ST	HOUSE #94333	R - Residential/Local	Minor Collector
SECOND ST	ST. HWY. 101	BOWMAN ST	R - Residential/Local	Minor Collector
SECOND ST	JACKSON ST	ST. HWY. 101	R - Residential/Local	Minor Collector
SHOPPING CENTER AV	LOWER HARBOR RD	W. HOFFELDT LN	RMaC - Rural Major Collector	Major Collector
SIXES RIVER RD	HOUSE #96624	END OF PAVEMENT	RMiC - Rural Minor Collector	Major Collector
SIXES RIVER RD	MILEPOST 7.0	HOUSE #96624	RMiC - Rural Minor Collector	Major Collector
SIXES RIVER RD	ST. HWY. 101	HOUSE #94714	RMaC - Rural Major Collector	Major Collector
SIXES RIVER RD	HOUSE #94714	MILE POST 7.0	RMaC - Rural Major Collector	Major Collector
VISTA DR	OLD MILL RD	END OF STRIPING	RMiC - Rural Minor Collector	None
W BENHAM LN	WENBOURNE LN	BOAT BASIN RD	RMaC - Rural Major Collector	Major Collector
W BENHAM LN	ST. HWY. 101	WENBOURNE LN	RMaC - Rural Major Collector	Major Collector
W HOFFELDT LN	ST. HWY. 101	END OF COUNTY ROAD	RMiC - Rural Minor Collector	Major Collector
WEDDERBURN LOOP RD	ST. HWY. 101 SOUTH EXIT	EASTERLY END OF DOYLE POINT RD	RMiC - Rural Minor Collector	None
WEDDERBURN LOOP RD	EASTERLY END OF DOYLE POINT RD	ST. HWY. 101 NORTH OFF-RAMP	RMiC - Rural Minor Collector	None
WINCHUCK RIVER RD	FIRE STATION, #00366	USFS RD #1107	RMaC - Rural Major Collector	Major Collector
WINCHUCK RIVER RD	LILES DR	FIRE STATION / HOUSE #00366	RMaC - Rural Major Collector	Major Collector
WINCHUCK RIVER RD	ST. HWY. 101	LILES DR	RMaC - Rural Major Collector	Major Collector
ZIMMERMAN LN	ST. HWY. 101	SHOPPING CENTER AVE	RMaC - Rural Major Collector	None

ATTACHMENT B – HDM URBAN CONTEXT CROSS SECTION GUIDANCES

HDM Urban Context Cross Section Guidance for State Highways

Design Element	Width (Feet)					
	Traditional Downtown/CBD	Urban Mix	Commercial Corridor	Residential Corridor	Suburban Fringe	Rural Community
Pedestrian Realm						
Frontage Zone	2-4	1	1	1	1	1
Pedestrian Zone	8-10	5-8	5-8	5-8	5-8	5-9
Buffer/Furniture Zone	0-6	0-6	0-6	0-6	0-6	0-5
Curb/Gutter	0.5-2	0.5-2	0.5-2	0.5-2	0.5-2	0.5-2
Transition Realm						
Separated Bicycle Lane (Curb Constrained)	7-8	7-8	7-8	7-8	7-8	7-8
On-Street Bicycle Lane (w/o Buffer)	5-6	5-6	5-6	5-6	6	5-6
Bicycle/Street Buffer	2-3	2-4	2-5	2-5	2-5	2-4
Right Shoulder (Travel Lane Adjacent to Curb)	0-2	0-2	0-4	0-4	0-6	0-6
On-Street Parking	7-8	8	N/A	N/A	N/A	8
Travelway Realm						
Travel Lane	11	11-12	11-12	11-12	11-12	11-12
Right-Turn Lane (w/Shy Distances)	11-12	11-12	12-13	12-13	12-13	11-12
Left-Turn Lane	11	11-12	12-14	12-14	12-14	11-12
Left Side / Right Side Shy Distance	0-1	0-1	0-1	0-1	0-1	0-1
Two-Way Left-Turn Lane	11-12	11-12	12-14	12-14	12-14	11-12
Raised Median – No Turn Lane (w/Shy Distances)	8-11	8-11	8-11	8-11	8-13	8-11
Left-Turn Lane w/Raised Curb Median/Separator (with 16" Separator & Shy Distances)	12-14	12-14	14-16	14-15	14-16	12-14

ATTACHMENT C – BRIDGE DATA

Scour Critical Bridges and Culverts in Curry County

Name	ID	Owner	Carries	Crosses	MP
Bridges					
Winchuck River Rd	20530	County	Winchuck River Rd	Cat Creek	6.00
Mill Creek	15C23	County	S Bank Chetco River Rd	Mill Creek	5.70
North Fork Chetco River	15C14	County	N Bank Chetco River Rd	N Fork Chetco River	5.30
Thomas Creek	08459	ODOT	US 101	Thomas Creek	347.78
Pistol River	15C33	County	Pistol River Loop Rd (Co. Rd 693)	Pistol River	0.30
Deep Creek	15C45	County	North Bank Pistol River Rd	Deep Creek	5.07
Upper Crook Creek	15C32	County	North Bank Pistol River Rd	Upper Crook Creek	0.07
Pistol River	08719	ODOT	US 101	Pistol River	339.10
Little South Fork Hunters Creek	15C17	County	Hunter Creek Rd	Little South Fork Hunters Creek	0.08
Hunters Creek	15C25	County	Hunter Creek Rd	Hunters Creek	3.80
Hunters Creek	15C24	County	Hunter Creek Rd	Hunters Creek	0.90
Hunter Creek	15C010	County	Hunter Creek Rd	Hunter Creek	0.20
Rogue River	01172	ODOT	US 101	Rogue River	327.70
Indian Creek	15C009	County	Jerry's Flat Rd	Indian Creek	0.70
Jim Hunt Creek	15C36	County	Jerry's Flat Rd	Jim Hunt Creek	5.20
Edson Creek	15C007	County	N Bank Rogue River Rd	Edson Creek	0.39
Edson Creek	15C006	County	N Bank Rogue River Rd	Edson Creek	0.10
Kimball Creek	15C37	County	Jerry's Flat Rd	Kimball Creek	8.10
Lobster Creek	15C16	County	N Bank Rogue River Rd	Lobster Creek	0.73
Greggs Creek	15C27	County	Ophir Rd	Greggs Creek	0.00
Euchre Creek	15C31	County	Ophir Rd (Co. Road 510)	Euchre Creek	0.10
Myrtle Creek	15C15	County	Arizona Ranch Rd (Co. Road 500)	Myrtle Creek	0.30
Arizona Beach	20962	State Park	Pedestrian Bridge	Myrtle Creek	0.00
Brush Creek	02382A	ODOT	US 101	Brush Creek	309.56
Bear Trap Creek	02386A	ODOT	US 101	Bear Trap Creek	308.84
Humbug State Park	21572	State Park	Vehicle Bridge	Creek	0.00
Humbug Mtn Park Trail	22713	State Park	Humbug Mtn Trail	Brush Creek	0.00
Brush Creek	07787	ODOT	US 101	Brush Creek	307.79
Brush Creek Trail (#3)	21515	State Park	Bike/ Pedestrian	Brush Creek	0.00
Brush Creek Trail (#2)	21516	State Park	Bike/ Pedestrian	Brush Creek	0.00
Brush Creek	07786	ODOT	Front Rd	Brush Creek	307.02
Brush Creek Trail (#1)	21514	State Park	Bike/ Pedestrian	Creek	0.00
Humbug Mt Ped Bridge	21005	State Park	Park Road	Brush Creek	0.00
Humbug Day Use	19783	State Park	Park Road	Brush Creek	307.02

Name	ID	Owner	Carries	Crosses	MP
Bridges					
Humbug Mt Trail Bridge	21518	State Park	Bike/ Pedestrian	Trail	0.00
Elk River	00902C	ODOT	US 101	Elk River	297.37
Sixes River	00901C	ODOT	US 101	Sixes River	295.75
Edson Creek	15C004	County	Sixes River Rd	Edson Creek	4.20
Crystal Creek	00928A	ODOT	US 101	Crystal Creek	295.49
Drainage Ditch/Access Rd	09875	ODOT	Access Rd Hwy 9	Drainage Ditch	295.27
Hwy 250 Culvert	03360	State Park	OR 250	Drainage	2.11
Willow Creek	15C12	County	Floras Lake Rd	Willow Creek (eastbound)	0.40
Floras Creek	09370	ODOT	US 101	Floras Creek	288.50
Floras Creek	15C19	County	Floras Creek Rd	Floras Creek	1.60
Floras Creek	15C21	County	Floras Creek Rd	Floras Creek	7.30
North Floras Creek	15C26	County	Floras Creek Rd	Floras Creek (N Fork)	8.90
Morton Creek	00912	ODOT	US 101	Morton Creek	286.61
Culverts					
Harris Creek	08417	ODOT	US 101	Harris Creek	355.19
Shigh Creek	08416	ODOT	US 101	Shigh Creek	354.53
Taylor Creek	08415	ODOT	US 101	Taylor Creek	354.06
Lone Ranch Creek	08433A	ODOT	US 101	Lone Ranch Creek	352.43
Bowman Creek	08451	ODOT	US 101	Bowman Creek	350.05
China Creek	08537	ODOT	US 101	China Creek	347.16
Spruce Creek	08536	ODOT	US 101	Spruce Creek	346.82
Horse Prairie Creek	08535	ODOT	US 101	Horse Prairie Creek	346.34
Wridge Creek	08488	ODOT	US 101	Wridge Creek	345.64
Miner Creek	08489	ODOT	US 101	Miner Creek	345.44
Miller Creek	08487	ODOT	US 101	Miller Creek	345.21
Hooskanaden Creek	08456	ODOT	US 101	Hooskanaden Creek	344.17
Whiskey Creek	08455	ODOT	US 101	Whiskey Creek	343.40
Burnt Hill Creek	08454	ODOT	US 101	Burnt Hill Creek	342.37
Hwy 9 Culvert	0P281	ODOT	US 101	Creek	338.13
Hwy 9 Culvert	0P280	ODOT	US 101	Creek	336.31
Riley Creek	03357A	ODOT	US 101	Riley Creek	329.00
Greggs Creek	07875	ODOT	US 101	Greggs Creek	318.58
Retz Creek	07781	ODOT	US 101	Retz Creek	304.35
Hwy 9 Culvert	0P435	ODOT	US 101	Creek	299.33
Hwy 9 Culvert	0P436	ODOT	US 101	Creek	298.90
Small Creek / Cattlepass	03361	State Park	OR 250	Cattlepass / Drainage	2.01
Swanson Creek	19090	ODOT	US 101	Swanson Creek	291.32